



AGENDA

SWALE JOINT TRANSPORTATION BOARD MEETING

Date: Monday, 20 June 2022

Time: 5.30 pm

Venue: Council Chamber, Swale House, East Street, Sittingbourne, ME10 3HT*

Membership:

Councillors Monique Bonney (Vice-Chair), Alastair Gould, Elliott Jayes, Julian Saunders, David Simmons, Paul Stephen and Eddie Thomas.

Kent County Council Members:

Kent County Councillors Mike Baldock, Cameron Beart, Andy Booth, Mike Dendor, Antony Hook and Rich Lehmann and John Wright (Chair).

Parish Council Members:

Kent Association of Local Council's representatives: Cllrs John Fassenfelt, Richard Palmer and Jeff Tutt.

Quorum = 5 (2 from each Council and 1 Parish representative).

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Information for the Public

*Members of the press and public may follow the proceedings of this meeting

live via a weblink which will be published on the Swale Borough Council website on Friday 17 June 2022.

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1. Confirmation of Chair and Vice-Chair

To confirm the Chair and Vice-Chair for the Municipal Year 2022/23.

2. Apologies for absence and confirmation of substitutes

3. Minutes

To approve the [Minutes](#) of the Meeting held on 28 February 2022 (Minute Nos. 630 - 641) as a correct record, subject to the following amendment to Minute No. 632 (Public Session) second paragraph, second sentence to be deleted "~~The petition contained almost 200 signatures.~~"

4. Declarations of Interest

Councillors should not act or take decisions in order to gain financial or other material benefits for themselves or their spouse, civil partner or person with whom they are living with as a spouse or civil partner. They must declare and resolve any interests and relationships.

The Chairman will ask Members if they have any interests to declare in respect of items on this agenda, under the following headings:

(a) Disclosable Pecuniary Interests (DPI) under the Localism Act 2011. The nature as well as the existence of any such interest must be declared. After declaring a DPI, the Member must leave the meeting and not take part in the discussion or vote. This applies even if there is provision for public speaking.

(b) Disclosable Non Pecuniary Interests (DNPI) under the Code of Conduct adopted by the Council in May 2012. The nature as well as the

existence of any such interest must be declared. After declaring a DNPI interest, the Member may stay, speak and vote on the matter.

(c) Where it is possible that a fair-minded and informed observer, having considered the facts would conclude that there was a real possibility that the Member might be predetermined or biased the Member should declare their predetermination or bias and then leave the room while that item is considered.

Advice to Members: If any Councillor has any doubt about the existence or nature of any DPI or DNPI which he/she may have in any item on this agenda, he/she should seek advice from the Monitoring Officer, the Head of Legal or from other Solicitors in Legal Services as early as possible, and in advance of the Meeting.

5. Public Session

Members of the public have the opportunity to speak at this meeting. Anyone wishing to present a petition or speak on this item is required to register with the Democratic Services Section by noon on Friday 17 June 2022. Questions that have not been submitted by this deadline will not be accepted. Only two people will be allowed to speak on each item and each person is limited to asking two questions. Each speaker will have a maximum of three minutes to speak.

Petitions, questions and statements will only be accepted if they are in relation to an item being considered at this meeting.

Part One - Reports for recommendation to Swale Borough Council's Policy and Resources Committee

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| 6. | Formal objections, comments, and indications of support to Traffic Regulation Order - Swale Amendment 34 (2022) | 5 - 26 |
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Part Two - Reports for recommendation to Kent County Council's Cabinet

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| 9. | Waiting Restrictions - Church Road and Tonge Road, Murston | 55 - 84 |
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| 11. | Update - A249 Key Street and Grovehurst Road junction Improvements | 87 - 98 |
| 12. | Presentation on New Drainage Processes | |
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| 14. | Highways Work Programme | 101 -
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| 15. | Progress Update Report | 129 -
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| | To consider the Progress Update which outlines progress made following recommendations and agreed action at previous meetings. | |
| 16. | ITEM FOR NOTING ONLY - Requests made by Councillors and Members of the Swale Joint Transportation Board | 131 -
132 |
| 17. | Date of Next Meeting | |
| | The next meeting will be held at 5.30 pm on Monday 5 September 2022. | |

Issued on Friday, 10 June 2022

The reports included in Part I of this agenda can be made available in **alternative formats**. For further information about this service, or to arrange for special facilities to be provided at the meeting, **please contact DEMOCRATIC SERVICES on 01795 417330**. To find out more about the work of the Swale JTB, please visit www.swale.gov.uk

**Chief Executive, Swale Borough Council,
Swale House, East Street, Sittingbourne, Kent, ME10 3HT**

SWALE JOINT TRANSPORTATION BOARD	Agenda Item:
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Meeting Date	Monday 20 th June 2022
Report Title	Formal Objections, Comments, and Indications of Support to Traffic Regulation Order – Swale Amendment 34 (2022)
Head of Service	Martyn Cassell, Head of Environment and Leisure
Lead Officer	Brett O’Connell (SBC)
Classification	Open

Recommendations	<p>Members are asked to note the comments of the recent formal consultation and recommend that: -</p> <p>The extension of the existing Residents’ Parking Scheme to include Edith Road, Faversham <u>either</u> be progressed <u>or</u> abandoned.</p>
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1. Purpose of Report and Executive Summary

- 1.1 This report provides details of objections, comments and indications of support received in relation to the recently advertised Traffic Regulation Order, Swale Amendment 34 2022, which covers the proposed extension of the Faversham Residents’ Parking Scheme to include Edith Road.

2. Background

- 2.1 In May/June 2021, an informal consultation took place with residents of Edith Road, Faversham, asking if they would like to be included in the current Residents’ Parking Scheme. Of the 32 properties consulted, a total of 19 responses were received, giving a response rate of 59%. Of the 19 responses, 9 (47%) supported the possible extension of the current scheme to include Edith Road and 10 (53%) objected to the extension. The results of that consultation were reported to the Swale Joint Transportation Board at their meeting in September 2021, where Members recommended that the extension to the scheme in Edith Road be progressed.
- 2.2 Following that recommendation, a second consultation with the residents of Edith Road, and two properties in The Mall who have vehicle accesses in Edith Road, took place in September 2021. The consultees were asked to comment on a

proposed draft layout design, which is included in Annex A. The comments received were considered and the design was amended and submitted to the Swale Joint Transportation Board in their meeting in December 2021, where Members recommended that the extension to the Residents' Parking Scheme to include Edith Road be progressed as per the amended design included in Annex B. Letters were sent out to the residents to notify them of the changes following the recommendation.

3. Issue for Decision

- 3.1 A Traffic Regulation Order has been advertised for the proposed extension of the Residents' Parking Scheme to include Edith Road, and the formal consultation took place between 4th March 2022 and 25th March 2022. A copy of the Public Notice associated with this Traffic Regulation Order is included in Annex C.
- 3.2 Following the formal consultation, we received 9 objections, 4 indications of support and 1 general comment from Kent County Council. Support for the proposals have been received. A copy of the formal objections, indications of support and comments received can be found in Annex D.
- 3.3 Comments from Ward Members/Town Council/Parish Councils

Faversham Town Council stated *“Following the formal consultation, Faversham Town Council support the proposed extension to the residents’ parking scheme to include Edith Road”*

4. Recommendation

- 4.1 Members are asked to note the comments of the recent formal consultation and recommend that the extension of the existing Residents' Parking Scheme to include Edith Road, Faversham **either** be progressed **or** abandoned.

5. Implications

Issue	Implications
Corporate Plan	Improving Community Safety through safer Highways.
Financial, Resource and Property	Cost of Advertising Made Order, Cost of Installing Lines and Signs on site. Resource to amend current administration of the Residents' Parking Scheme to include permit eligibility for Edith Road.
Legal and Statutory	Sealing of Traffic Regulation Order by Kent County Council.
Crime and Disorder	None at this stage.

Risk Management and Health and Safety	None identified at this stage.
Equality and Diversity	None identified at this stage.
Sustainability	None identified at this stage.
Health Implications	The introduction of a Residents' Parking Scheme in Edith Road, Faversham, may improve the mental wellbeing of some residents who currently feel that on-street parking capacity is reduced due to parking by non-residents, and may allow them to park closer to their properties, reducing the distance to walk from their vehicles. However, stress to some residents may be increased by the introduction of the Scheme if they own more vehicles than they can accommodate with the Permit Scheme, forcing them to park further from their properties. The mental wellbeing of residents in adjoining roads may be impacted by the displacement of vehicles into their roads, reducing available on-street parking capacity. Some residents may also find the annual cost of the permits to be an additional financial burden.

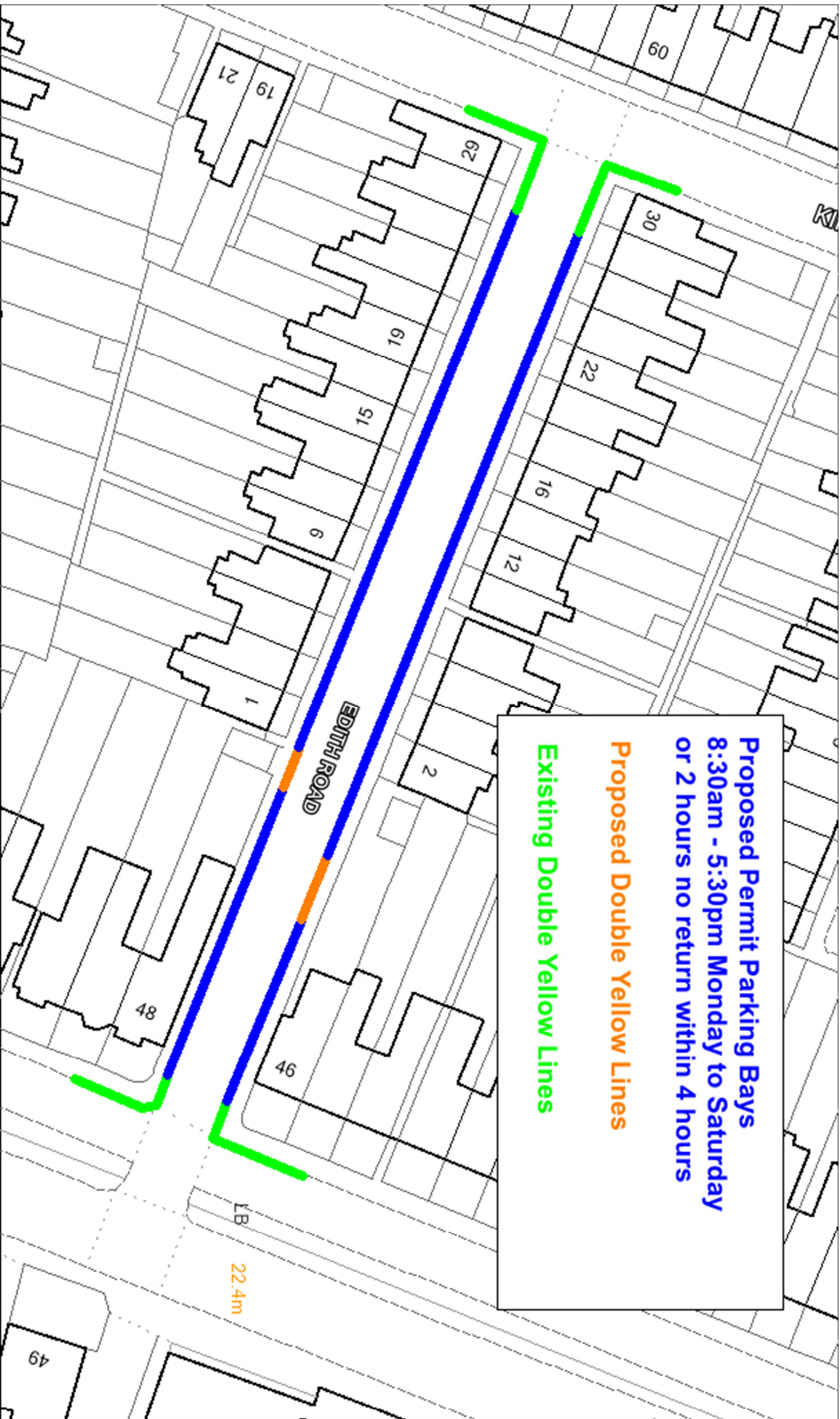
6. Appendices

- 6.1 Annex A – Copy of the original design layout plan
Annex B – Copy of the amended design layout plan
Annex C – Copy of the Public Notice advertising the Order
Annex D - Copy of Formal Objections, Indications of Support & Comments

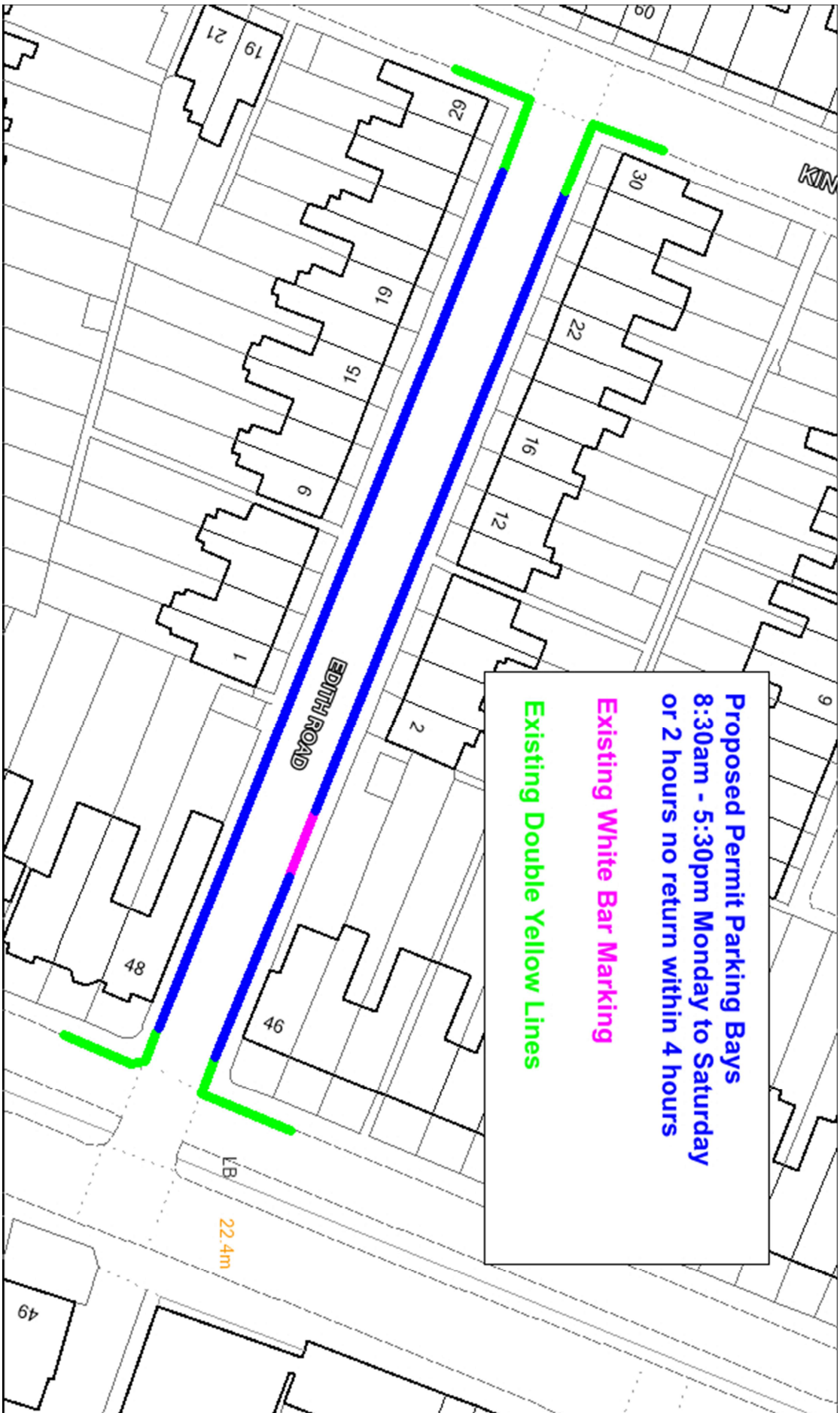
7. Background Papers

- 7.1 None

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PUBLIC NOTICE



**THE KENT COUNTY COUNCIL (VARIOUS ROADS, BOROUGH OF SWALE)
(WAITING RESTRICTIONS AND STREET PARKING PLACES)
(AMENDMENT NO.34) ORDER 2022**

EDITH ROAD, FAVERSHAM

Kent County Council propose to make the above Order, the effect of which will introduce a number of new or amend some existing parking or waiting restrictions.

In Edith Road, Faversham, the effect of the Order will be as follows:-

It is proposed to extend the existing area of the Faversham Residents' Parking Scheme to cover the full length of Edith Road, through the introduction of Residents' Parking Bays.

Full details are contained in the draft Order which, together with a statement of the Council's reasons for proposing to make the Order, a map and a copy of any Orders which will be amended by the proposed Order may be examined on our online consultation page at www.swale.gov.uk, or further information can be requested by e-mailing engineers@swale.gov.uk.

If you have any questions concerning the proposed Order or require further information, please contact Swale Borough Council (01795 417061) during normal office hours.

If you wish to object to a proposed Order you should send the grounds for your objection in writing to Engineering Team, Leisure & Technical Services, Swale Borough Council, Swale House, East Street, Sittingbourne, Kent, ME10 3HT by noon on Friday 25th March 2022. Indications of support are welcome.

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Formal Objections, Indications of Support and Comments – Extension of Residents’ Parking Scheme, Edith Road, Faversham

Annex D

Response No.	<u>Objections</u>
1	<p>The parking order to Edith Road, as a resident of ... Edith Road of 24 years I wholly object to any form of resident permit scheme. The perception that parking allocation solely of residents will provide an advantage whilst family households will be charged for this privilege. In a recent survey, parking spaces during the potential restricted period were allocated to residents, the perception it attracts commuter traffic is not substantiated.</p> <p>Regardless of the counsellor's ill-conceived decision of this order from KKC, today families are facing financial hardship and they now wish to inflict yet another charge that has not been verified nor evidence-based providing any value or benefit to residents. In fact, this will further increase pressure on neighbouring roads such as Kingsnorth Road, where there are no restrictions during the week.</p> <p>The plans have no consideration of the last resident vote, democratic system with a counsellor breaching our confidence as a serving officer. Frankly waste of public spend and time.</p> <p>It is my understanding that the plan, including yellow lines, will further reduce parking into the road plan will only escalate the problem. So I urge you to consider what is right and represent the resident voice, and withdraw the plans.</p>
2	<p>FORMAL OBJECTION Dear Sirs,</p> <p>I have lived in Edith Road for the best part of 18 years, and yes, parking in the road is sometimes difficult, especially in the evenings but on the most part it is not a problem.</p> <ol style="list-style-type: none"> 1. Several years ago, the council wanted to add Permit parking to Edith Road, Kingsnorth Road, and Belmont Road. This resulted in double yellow bands being put in place on the corners, and the prospect of losing 13 or so parking spaces over all 3 roads. It was hotly contested by many of the residents, and as such was not put in place. 2. The current idea was borne out of the need for some of the residents of Edith Road to be able to park outside their homes?! Sailing under some kind total misapprehension that a permit will grant them a space outside their property !!

	<ol style="list-style-type: none"> 3. A residents parking permit does not specify a specific space or even a specific road, simply a permit to park in the zone. 4. There are 31 houses in Edith Road, with an average space for 31 cars:(Road length 226 metres - average space for car 6 metres 226/6 -37 cars.Double yellow lines -6 car spaces. Therefore, there are 31 parking spaces, providing people park politely! Some households have 2 or more cars and spend a lot of their time moving their cars to create a space for their 2nd vehicle and taking up 3 spaces! 5. It has worked perfectly well up to now, and yes, we get some commuters, people from the Abbey school on London Road, and people who work in The Mall shops, and on weekends, people parking to go into town to avoid paying car parking fees! The other factor is that vehicles owned by residents in Zone B (the Mall) already park in Edith Road some to avoid their cars being covered in bird droppings and sap from the trees. 6. However, on any given day there are more than 7/8 parking spaces, and it does not become full. 7. The road is generally empty most days, and only becomes full at night as people return from work. 8. I fail to understand as to why there is the need to make it a parking control zone, as much of the time you can park in the road. There is no benefit to be gained from making Edith Road a residents permit area, as the problem (if there is one?) will be shifted to Kingsnorth and Belmont. If the scheme is to be adopted, then it should be necessary to include all surrounding roads. 9. The surrounding roads do need to be under the permit scheme for this to work efficiently! 10. People from all over Faversham can still park here if they have a parking permit. Especially those who use the road to commute . 11. Anyone can park from 16.00 until 10.00am without a permit, and in the middle of the day there is always parking. At 02.00am there is no parking available. 12. It was never explained to any household that there is only 2 permits available to each household, and there are several that have 3 cars, due to children still living at home. 13. There is actually not enough space for everyone who has a car and subsequently buys a permit to park within Edith Road. <p>On my points above, I strongly contest the use of parking permits in Edith Road, and feel there will be no benefit to anyone in the road except the council gaining more income.</p>
3	<p>Dear Sirs,</p> <p>I have lived in Edith Road for the best part of 36 years, and yes, parking in the road is sometimes difficult, especially in the evenings but on the most part it is not a problem.</p> <ol style="list-style-type: none"> 1. Several years ago, the council wanted to add Permit parking to Edith Road, Kingsnorth Road, and Belmont Road. This resulted in double yellow bands being put in place on the corners, and the prospect of losing 13 or so parking spaces over all 3 roads. It was hotly contested by many of the residents, and as such was not put in place. 2. The current idea was borne out of the need for some of the residents of Edith Road to be able to park outside their homes?!

3. A residents parking permit does not specify a specific space or even a specific road, simply a permit to park in the zone.
4. There are 31 houses in Edith Road, with an average space for 31 cars:
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Double yellow lines -6 car spaces. Therefore, there are 31 parking spaces, providing people park politely! Some households have 2 or more cars and spend a lot of their time moving their cars to create a space for their 2nd vehicle and taking up 3 spaces!
5. It has worked perfectly well up to now, and yes, we get some commuters, people from the Abbey school on London Road, and people who work in The Mall shops, and on weekends, people parking to go into town to avoid paying car parking fees! The other factor is that vehicles owned by residents in Zone B (the Mall) already park in Edith Road some to avoid their cars being covered in bird droppings and sap from the trees.
6. However, on any given day there are more than 7/8 parking spaces, and it does not become full.
7. The road is generally empty most days, and only becomes full at night as people return from work.
8. I fail to understand as to why there is the need to make it a parking control zone, as much of the time you can park in the road. There is no benefit to be gained from making Edith Road a residents permit area, as the problem (if there is one?) will be shifted to Kingsnorth and Belmont. If the scheme is to be adopted, then it should be necessary to include all surrounding roads.
9. The surrounding roads do need to be under the permit scheme for this to work efficiently!
10. People from all over Faversham can still park here if they have a parking permit. Especially those who use the road to commute .
11. Anyone can park from 16.00 until 10.00am without a permit, and in the middle of the day there is always parking. At 02.00am there is no parking available.
12. It was never explained to any household that there is only 2 permits available to each household, and there are several that have 3 cars, due to children still living at home.
13. There is actually not enough space for everyone who has a car and subsequently buys a permit to park within Edith Road. On my points above, I strongly contest the use of parking permits in Edith Road, and feel there will be no benefit to anyone in the road except the council gaining more income.

4

Dear Sir,

Having lived at this address for 30 years we can see no evidenced argument or justifiable reason for the imposition of a residents' parking permit scheme in Edith Road. Parking has always been a challenge and certainly it is rare that residents are able to park outside of our homes but

The reasons for objection are listed below.

1. Edith Road street length is 113 metres x 2 = 226 metres of road-side.
2. The average space required for parking a small family car is 6 metres: $226 \div 6 = 37$ cars total capacity. Note that larger vehicles owned by residents are also regularly parked in the road.
3. Existing double yellow lines at the corners of Edith Road total approximately 6 car spaces. Total space available is thus for 31 cars.
4. There are currently 31 residential addresses in Edith Road including the flats at number 4.
5. A number of households have two or three vehicles and from time to time households may have more especially where adult children are living with parents or residents are self-employed with business vehicles.
6. In other words, there simply is not enough room in the street to accommodate all the residents' vehicles.
7. There is an erroneous claim in some quarters that residents parking is significantly impeded by commuters. This simply is not true. There are one or two vehicles which may be able to find a space to park in the road early in the day where a resident has left for work earlier but otherwise such a problem does not exist. During the day, there are always a number of empty spaces available for residents to leave and return and to facilitate service vehicles attending households.
As residents return from work in the early evening the road quickly fills. By 21:00 and throughout the night the road is filled to capacity. Clearly this is not due to commuters but

simply to residents' vehicles. This has always been the case and your records will show the results of your overnight survey when this matter was last raised in 2011. Anyone returning home later in the evening / early morning will inevitably have to park in Canute Road or even further away. A permit scheme will thus not solve the problem.

The situation is even more obvious over weekends when there are no commuters travelling yet the street is full.

Several photographs are appended to this letter to illustrate the various conditions.

8. The parking situation is additionally exacerbated by vehicles owned by residents from other roads in Zone B who are forced to already park in Edith Road when over-subscribed scheme in The Mall spaces are full.

9. A residents' parking permit does not specify a space or road; it is simply a permit to park in the zone. There are as you know currently numerous roads in Faversham whose residents are eligible to purchase Zone B permits.

Finally, I note that the order consultation notice posted in the road requires consultees to submit responses by post and not email. This is contrary to communications best practice within national and local government. One hopes that those submissions made by email will also be properly considered along with the postal submissions.



Thursday 3rd March @ 10 :00 hrs.



Sunday 20th March @ 10:00 Hrs

5 and 6

Dear Sir/Madam

I am wishing to express my objection to the proposed residential parking restrictions for Edith Road. Although I live in Kingsnorth Road the proposal will have a direct impact on my Road.

Firstly anyone from Edith Road will rightly be able to park in Kingsnorth Road if they need to, but I will not be allowed to park in their road if there is a space available. Some residence think that they will have a space outside their house in future if they have residential parking permit and that it is the commuters that are causing the parking problem. This is not true, as it is not the commuters that are the problem but the sheer number of residential vehicles in our area and we are all to blame for this being multi car houses.

The only thing that will happen will be to move the problem to another street, namely Kingnorth Road and not solve anything. All it will mean is that the residents in my road will request residential parking also, so we will be back to square one apart from having to pay £60 per car to park where we are now.

The top and bottom of it is that there are too many cars trying to park on the street in our area, restrictions will not solve this, so leave well alone and let us carry on as before.

To Engineering

an against part in
bays in Edith Road. It will not
solve the problem

The problem is too many cars
In 1973 when I moved into the
road there was only one other
car, as times have changed most
people have two cars and some
have three vehicles

We have cars from the Mall and
London Road, so putting bays will
not solve the problem but just
make more money for the Council

We are writing this letter in response to the proposed change in parking arrangements in EDITH ROAD.

As a long term resident in Edith Road for over 40 years the phrase comes to mind of "why fix something that isn't broken"!!!

Permit Parking and the cost incurred for these does not even guarantee a space or spaces outside your own home, sadly some residents think it does. The cars without permits have to find somewhere else to park which obviously would make neighboring roads ie Kingsnorth, Belmont and Canute even more congested than they already are.

	<p>We are a friendly area to live and a lot of people are worried it would cause upset between households. Nobody wants this at all.</p> <p>So all in all we do not understand what you want to achieve by implementing this course of action that and strongly disagree with the proposal.</p>
9	<p>Proposal to extend Residents Parking Scheme to Edith Road, Faversham</p> <p>I wish to make an objection to the above scheme on the following grounds:</p> <ol style="list-style-type: none"> 1 If the scheme was introduced it would exacerbate the current situation where a number of residents in Edith Road already routinely park in Kingsnorth Road because there is simply not enough space for their vehicles. 2 A number of residents in Edith Road have more than one vehicle and the introduction of parking permits is not going to provide them with guaranteed parking and they and their neighbours are still in the same position. The possible offer of more than one permit per household would only make things worse. 3 As a consequence of the above and as a resident of Kingsnorth Road we also are not guaranteed to park near our house or indeed in Kingsnorth Road itself. With the influx of Edith Road car owners and others from the A2 who park in Kingsnorth Road we frequently have to park in Canute Road if space is available. <p>Put simply there are just too many cars!</p>

Response No.	Indications of Support
1	I hope you still have me supporting this parking scheme, my original email for support was sent on the 1st June.
2	Please could you accept this email as support for the parking scheme for Edith Road, I have previously emailed in when in the consultation period for support so if this email is too late then please us my original support email to you from this email address.
3	<p>We are responding to the recent letter regarding the proposed extension of the residents' parking scheme to Edith Road in Faversham. We at Edith Road are both in complete support of the proposal to introduce a parking permit scheme to the street.</p> <p>We have lived in Faversham for over 20 years and have noticed that parking in streets near the station where the permit was required, such as Athelstan Road were much easier to find parking than in those that didn't, such as Edith Road.</p> <p>Whilst those in opposition may dispute it, it is an undeniable and observed fact by residents that non-residents use the street for free parking during the 'working day', which can last from 7am through to 7pm. Teachers at the Abbey School, commuters and people who work in town have all been observed by myself and other residents using the street to park their cars to avoid station parking charges or town parking charges. Whilst one can hardly blame people for taking advantage of this opportunity it has an unfortunate effect that makes parking for residents extremely difficult, especially at the staggered end of the working day, between 5.30 and 7pm.</p> <p>To this extent, we have already bought a parking permit as we often find ourselves having to park on the Mall. I know that our neighbours at have done the same for similar reasons. For us, it is an accepted and necessary part of living on a Victorian street. Parking on the Mall is never an issue and to our mind that is because it has a permit scheme. Parking on Edith or Kingsnorth Roads is ridiculously difficult and we believe it is because there is no permit scheme.</p> <p>Some of the counter arguments for introducing this scheme include the assertions that the parking problem is caused by households having more than one car. Whilst this may be the case in some houses, it is not in ours, and indeed out of all our neighbours, I don't believe any of them has more than one car. I can't speak for the rest of the street.</p> <p>They also argue that anyone with a permit could park on the street, but that applies now, and a quick survey of most of the cars on our street will show that few have permits, are those that do are residents of the street.</p> <p>During lockdown when no-one was really driving we never had an issue finding a space when we went to the supermarket, which proves there is adequate space for residents. It is on returning from work that space becomes an issue.</p> <p>We heartily endorse this scheme and hope that the parking permit scheme will be extended to Edith Road.</p>
4. Faversham Town Council	Following the formal consultation, Faversham Town Council support the proposed extension to the residents' parking scheme to include Edith Road

Response No.	General Comments
Kent County Council	Thank you for the opportunity to view the documents for Amendment 34, KCC has no observations or comments to make.

SWALE JOINT TRANSPORTATION BOARD	Agenda Item: 7
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Meeting Date	Monday 20 th June 2022
Report Title	Formal Objections to Traffic Regulation Order – Swale Amendment 35 2022
Head of Service	Martyn Cassell, Head of Environment and Leisure
Lead Officer	Mike Knowles (SBC)
Classification	Open

Recommendations	<p>Members are asked to note the formal objections, indications of support and comments received to the advertised Traffic Regulation Order and recommend that:-</p> <p>(1) the proposed double yellow lines on the junction of Power Station Road and William Rigby Drive, Halfway, be progressed;</p> <p>(2) the proposed double yellow lines outside Sheerness Working Men’s Club in Queenborough Road, Halfway, be progressed.</p>
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1. Purpose of Report and Executive Summary

- 1.1 This report provides details of objections, comments and indications of support received in relation to the recently advertised Traffic Regulation Order, Swale Amendment 35 2022, which covers various amendments to on-street waiting restrictions in the Swale area. The proposals detailed in this report have been requested by the County Member for the area, to be funded through the County Members’ Grant Scheme.

2. Background

- 2.1 A Traffic Regulation Order has been drafted for various proposed amendments to on-street waiting restrictions in Swale, and the formal consultation took place between 4th March 2022 and 25th March 2022. Extracts from this Order where objections and comments have been received can be found in Annex A. A Statement of Reason summarising the relevant contents of the Order can be found

in Annex B. A number of formal objections, comments and indications of support, have been received to some of the proposals in the Traffic Order, and these are discussed below.

3. Issue for Decision

- 3.1 A copy of the formal objections, indications of support and comments received can be found in Annex C, and plans for each of these areas can be found in Annex D.

(1) Proposed Double Yellow Lines – Power Station Road/William Rigby Drive, Halfway

- 3.2 The County Member for the area has requested the installation of double yellow lines on the junction of Power Station Road and William Rigby Drive in Halfway. This follows issues with vehicles parking close to and around the junction, obstructing sightlines and hindering the safe movement of vehicles. The proposed double yellow lines are being funded through the County Members' Highway Grant Scheme.
- 3.3 During the formal consultation process, we received one formal objection and one indication of support. The objector supported the proposals but only if additional double yellow lines were installed opposite the junction of William Rigby Drive, to prevent vehicles parking on the opposite side of the road. As any increase to the originally advertised proposals would require the drafting of another Traffic Regulation Order, it is suggested that the original proposals be implemented, and the parking situation monitored.
- 3.4 Ward & County Member Comments: One of the County Members has provided the following comments regarding the proposals: *"This proposal was put forward following direct consultation with residents and I therefore welcome the comments and suggestion put forward that are being treated as an objection. I support the officers recommendation to implement the original proposals and monitor the situation to see if further works may be required in the future."*

(2) Proposed Double Yellow Lines – Outside Sheerness Working Men's Club, Queenborough Road, Halfway

- 3.5 The County Member for the area has requested the installation of double yellow lines outside of Sheerness Working Men's Club, following issues with vehicles parking close to the entrance/exit of the club's car park. These vehicles severely reduce the visibility for vehicles exiting the car park, and at least one road traffic accident has been reported.
- 3.6 During the formal consultation, we received one formal objection and a total of 4 indications of support. One indication of support was accompanied by a petition containing 52 signatures supporting the implementation of the restrictions. The objector stated that although the section of road where the proposed restrictions was small it provided much needed parking for up to 5 vehicles in an area where parking was already limited.

3.7 At the request of the County Member, the views of the local PCSO were sought, and he kindly provided the following comments: *“I am aware of the issues surrounding the area, although I have not seen anything untoward on my patrols. However, as there is a crossing very nearby, I would welcome anything that keeps lines of sight clear to make it safer for the general public. Of course, if the lines are put down, they would need to be patrolled on a regular basis. Unfortunately, I cannot ticket vehicles for parking issues, so this would have to be dealt with by the Council’s team.”*

3.8 Ward & County Member Comments: One of the County Members has provided the following comments in relation to the proposals: *“This proposal was put forward following direct consultation with residents and is well supported. The parking situation at this location has become of serious concern and I support the officers recommendation to implement.”*

(3) Proposed Double Yellow Lines – Holmside Avenue/Queenborough Road, Halfway

3.9 The County Member for the area requested the installation of double yellow lines on the junction of Holmside Avenue and Queenborough Road in Halfway, to tackle problems with vehicles parking close to the junction and hindering the safe passage of vehicles.

3.10 During the formal consultation, a total of 4 indications of support were received. As there were no formal objections received to the proposals there is no recommendation required from the Board, and the comments have been added to this report for information only.

4. Recommendation

4.1 Members are asked to note the formal objections, indications of support and comments received to the advertised Traffic Regulation Order and recommend that:-

(1) the proposed double yellow lines on the junction of Power Station Road and William Rigby Drive, Halfway, be progressed;

(2) the proposed double yellow lines outside Sheerness Working Men’s Club in Queenborough Road, Halfway, be progressed.

5. Implications

Issue	Implications
Corporate Plan	Improving Community Safety through safer Highways.

Financial, Resource and Property	Cost of Advertising Made Order, Cost of Installing Lines and Signs on site.
Legal and Statutory	Sealing of Traffic Regulation Order by Kent County Council.
Crime and Disorder	None at this stage.
Risk Management and Health and Safety	None identified at this stage.
Equality and Diversity	None identified at this stage.
Sustainability	None identified at this stage.
Health Implications	<p>The introduction of double yellow lines to improve sightlines and vehicle movements could have a positive impact on the mental health of drivers by reducing stress levels and potential incidents of road rage.</p> <p>However, where on street parking capacity is limited there may be some negative mental health effects on residents who may be forced to park further away from their properties, potentially increasing the distance to walk at night.</p>

6. Appendices

- 6.1 Annex A – Extract from Traffic Regulation Order Swale Amendment 35 2022
Annex B – Extract of Statement of Reason
Annex C – Copy of Formal Objections, Indications of Support & Comments
Annex D – Plans of Proposals Receiving Objections and Support

7. Background Papers

- 7.1 None

**THE KENT COUNTY COUNCIL (VARIOUS ROADS, BOROUGH OF SWALE)
(WAITING RESTRICTIONS AND STREET PARKING PLACES)
(AMENDMENT No.35) ORDER 2022**

FORMAL OBJECTIONS/SUPPORT/COMMENTS RECEIVED

OBJECTION 1 – POWER STATION ROAD/WILLIAM RIGBY DRIVE

OBJECTION 2 – WORKING MENS CLUB, QUEENBOROUGH ROAD

SUPPORT 1 – POWER STATION ROAD/WILLIAM RIGBY DRIVE

SUPPORT 2 – WORKING MENS CLUB, QUEENBOROUGH ROAD

SUPPORT 3 – WORKING MENS CLUB, QUEENBOROUGH ROAD

SUPPORT 4 – WORKING MENS CLUB & HOLMSIDE AVENUE, QUEENBOROUGH RD

SUPPORT 5 – WORKING MENS CLUB, QUEENBOROUGH ROAD

SUPPORT 6 – HOLMSIDE AVENUE/QUEENBOROUGH ROAD

SUPPORT 7 – HOLMSIDE AVENUE/QUEENBOROUGH ROAD

SUPPORT 8 – HOLMSIDE AVENUE/QUEENBOROUGH ROAD

The Kent County Council, acting as the local traffic authority and in exercise of its powers under sections 1(1), 2(1) to (3), 3(2), 4(1) and (2), 32(1), 35(1), 45, 46, 49 and 53 of the Road Traffic Regulation Act 1984, ('the Act') and of all other enabling powers, and after consultation with the chief officer of police in accordance with Part III of Schedule 9 to the Act, propose to make the following Order:-

A - This Order may be cited as "The Kent County Council (Various Roads, Borough of Swale) (Waiting Restrictions and Street Parking Places) Amendment No.35 Order 2022" ("this Order") and shall come into force on the xx day of xxxxx 2022.

B - The "Kent County Council (Various Roads, Borough of Swale) (Waiting Restrictions and Street Parking Places) (Consolidation) Order 2019" ('the 2019 Order') shall have effect as though -

In the Schedules to the 2019 Order

FIRST SCHEDULE

Roads in Minster-in-Sheppey in the Borough of Swale

Holmside Avenue, Halfway

The following shall be inserted in the First Schedule of the 2019 Order (*No Waiting At Any Time*) in the correct alphabetical sequence:-

HOLMSIDE AVENUE, HALFWAY

On both sides, from a [redacted] with the southern kerbline of Queenborough

Road for a distance of 14 metres in a southerly direction

SUPPORT 4, 6, 7, 8

Power Station Road, Halfway

The following shall be inserted in the First Schedule of the 2019 Order (*No Waiting At Any Time*) in the correct alphabetical sequence:-

POWER STATION ROAD, HALFWAY

On the southern side, from a point in line with the western kerbline of Band Way to a point 25 metres east of the centre of the Junction of William Rigby Drive

OBJECTION 1

SUPPORT 1

Queenborough Road

The following shall be inserted in the First Schedule of the 2019 Order (*No Waiting At Any Time*) in place of the existing entry:-

QUEENBOROUGH ROAD

(1) On the northern side

(a) from the Junction with Halfway Road for a distance of 69 metres in a westerly direction;

(b) between the boundary of 28/26 Queenborough Road and the boundary of 16/14A Queenborough Road;

(c) from a point opposite the boundary of St Peter's Church and 99 Queenborough Road to a point 5 metres west of the boundary of 162/164 Queenborough Road.

(2) On the southern side

(a) from the Junction with Halfway Road to a point 17 metres west of the Junction with Southdown Road;

(b) between points 10 metres west and 10 metres east of the Junction with Queenborough Road (cul-de-sac fronting 27-45 Queenborough Road);

(c) from a point in line with the boundary of St Peter's Church and 99 Queenborough Road to point opposite the boundary of 162/164 Queenborough Road;

(d) from a point in line with the western boundary of Sheerness East Working Men's Club for a distance of 39 metres in an easterly direction

OBJECTION 2

(e) between points 13 metres west and east of the centre of the Junction of
Holmside Avenue

SUPPORT 4, 6, 7, 8

(3) On both sides of the cul-de-sac fronting 27-45 Queenborough Road for a distance of 10 metres from the Junction with Queenborough Road.

William Rigby Drive, Halfway

The following shall be inserted in the First Schedule of the 2019 Order (*No Waiting At Any Time*) in the correct alphabetical sequence:-

WILLIAM RIGBY DRIVE, HALFWAY

On both sides, from a point in line with the southern kerblines of Power Station Road, to a point in line with the northern boundary of No. 1 William Rigby Drive

**OBJECTION 1
SUPPORT 1**

Given under the Common Seal of the Kent County Council

This xx day of xxxxxx 2022

THE COMMON SEAL OF THE
KENT COUNTY COUNCIL was
hereunto affixed in the
presence of:-

Authorised Signatory

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STATEMENT of REASON



**THE KENT COUNTY COUNCIL (VARIOUS ROADS, BOROUGH OF SWALE)
(WAITING RESTRICTIONS AND STREET PARKING PLACES)
(AMENDMENT NO.35) ORDER 2022
Road Traffic Regulation Act 1984**

To facilitate the safe passage of vehicles, it is proposed to install double yellow lines in Queenborough Road, Halfway on the Isle of Sheppey, around the junction of Holmside Avenue and outside Sheerness Working Men's Club, and also on the junction of Power Station Road and William Rigby Drive, Halfway.

For the following purposes:

- **To avoid danger to persons or other traffic using the road or any other road or to prevent the likelihood of any such danger arising;**
- **To facilitate the passage on the road or any other road of any class of traffic (including pedestrians).**

Dated 2nd February 2022
MIKE KNOWLES

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(1) Proposed Double Yellow Lines – Power Station Road/William Rigby Drive, Halfway

1 Formal Objection & 1 Indication of Support

Formal Objection (TRO Objection 1)

Dear Sir/Madam

I have today received a letter from your regarding the following:

Traffic Regulation Order - Swale Amendment 35 2022

Proposed Double Yellow Lines - Power Station Road / William Rigby Drive

I do not wish to object against the yellow lines that you are proposing but I do object to these being added, unless you also add additional double yellow lines opposite the junction as well (please see attached photo of amended proposal).

Lines opposite are necessary as lorries, vans and cars continuously park opposite the junction, which causes near collisions with traffic coming from the golf course and if you limit parking they will cause more obstruction by parking there more (please see photo attached showing van parked there overnight).

Regards



Indication of Support (TRO Support 1)

Good Evening,

Please put my vote forward for the proposal of double yellow lines along the above road.

This Junction has been a dangerous area since the new estate opposite was built & commercial Vehicles park along it obscuring on coming traffic.

There are no speed humps from the golf club along Power Station Road, meaning cars drive up the road with speed - while cars are trying to edge out of William Rigby Drive cannot see past the commercial vehicles on the left, also trying to check cars coming from the golf club. It then leaves no room once out of WRD to pull in if there is on coming traffic due to the vehicles parked on the left.

Also could I suggest perhaps lengthening the lines up to the crossing? Without this it currently makes it impossible for parents to see on coming traffic while they are trying to cross the road with their children. It is a Hazard for those of us trying to get out of the WRD estate & I'm surprise there hasn't been more accidents.

I have supplied photos of the selfish parking. [Sample of submitted photos below]



(2) Proposed Double Yellow Lines – Outside Sheerness Working Men’s Club, Queenborough Road

1 Formal Objection & 4 Indications of Support

Formal Objection (TRO Objection 2)

*I live at number ** Queenborough Road and own 1 van that is away all week and only at our property on weekends*

This notice that I have only just seen today is going to cause many parking problems to an already problem area

Most of the houses along this bit of road own 2 or 3 vehicles already causing people to park away from there houses all the way along .. the spill from it also effects the old peoples bungalows across the road as at night this caldesac is rammed with cars restricting access to much needed emergency services

I know this area between the gates is only small but it provides much needed parking for up to 5 vehicles

Before the club was used as a covid centre they were ok with residents parking in there car park. The use as a covid centre put pay to that and I understand. But since then the club banned resident parking .. (fair its there property)

And installed gates again fair .

But this planning notice and the lines that will be put there will cause so many parking problems more so than before

Not only that . People on there way to work stop there briefly to get there papers lunch etc from the queens news shop so it will effect there buisness aswell

The club have had there own way with the banned use of the car park and adding the gates . Please don't take the parking away there aswell i beg you

First Indication of Support (TRO Support 2)

Dear Sir/Madam,

I would like to comment on the above by stressing that I fully support the proposal.

*Parked vehicles along this stretch of highway seriously compromise sight lines and potentially make it hazardous to pull out onto the Queenborough Road from my property (**, Queenborough Road). Also it makes it difficult for pedestrians both crossing the road and walking along the pavement as these vehicles are usually parked partially on the pavement.*

If it proves, having completed the proposed installation, that vehicles park outside of both No's 57 and 59, hence replicating restricted sight lines etc., It would then be beneficial if the proposed double yellow lines were extended up to and in front of No. 59, in order to prevent this hazard.

Second Indication of Support (TRO Support 3)

Dear sir,

I would like to register my support for the double yellow lines outside sheerness east, Halfway.

Third Indication of Support (TRO Support 4)

Likewise, the double yellows outside the Sheppey East Sports Club are desperately needed.... Some days you can't walk along the pavement there because of vans parked half on the pavement and half in the road. Also, there is a bus stop opposite, an if a bus is stopped and vans parked, no one can move.

It's just dangerous to allow parking there.

Pleas get these yellow lines sorted asap.. We need them desperately before a serious accident occurs.

Fourth Indication of Support (TRO Support 5) (Includes Petition Containing 52 Signatures)

With reference to our telephone conversation on Thursday 10th March about the parking problems outside Sheerness East Club, Halfway, please find a list of people linked to the social activities of the Club who are in favour of double yellow lines.

Yesterday, my husband and I drove to the Club at 5.00pm [.....] and there were 3 large vans and 2 cars parked on the pavement by the wall outside the Club car park.

These vehicles were directly opposite a bus stop, very near the pedestrian crossing and almost opposite to Western Avenue which is the main road to the local Cemetery.

It is obvious to a lot of people that double yellow lines could go a long way to solve this hazard and hopefully prevent an accident.

Thank you for your time.

[Letter included a petition containing 52 signatures headed as follows]

“A consultation has been taking place at Swale Borough Council regarding the parking outside Sheerness East Club, Halfway. It has been noted that two vehicles have been regularly parking between the Exit/Entrance to the Club Car Park, sometimes these vehicles are parked on the pavement causing a problem for invalid scooters, wheelchairs, pushchairs, etc.

Club members exiting the car park in a car have experienced difficulties trying to negotiate their exit with these vehicles blocking their vision of oncoming traffic on a very busy road.

Furthermore, these vehicles are also close to the Zebra Crossing and almost opposite the road leading to the local Cemetery.

If you have experienced any of these problems and think that double yellow lines between the Exit/Entrance to the Club Car Park would help to solve these issues, please sign your name and postcode on this petition.”

(3) Proposed Double Yellow Lines – Holmside Avenue/Queenborough Road, Halfway

No Objections & 4 Indications of Support (For Information Only, No Recommendation Required)

First Indication of Support (TRO Support 6)

As a resident of Rosemary Ave this junction is my way of getting to and from my home. Negotiating this junction - getting both in and out of it - can be incredibly challenging due to the inconsiderate parking that takes place in Holmside right up to the corner of the junction. It is not helped by Holmside being a relatively narrow residential road with 'square' corners at the intersection. This means that if someone is parked right on the corner when you are trying to turn left onto Queenborough Rd, not only are you forced to approach the junction in the opposing lane but you are effectively having to pull out over the centre line in the main road to avoid making contact with the parked car.

At peak times the bad parking at this junction can cause absolute chaos from all directions. Common sense dictates that double yellow lines are required at this location.

Please accept my absolute support in favour of this proposal.

Second Indication of Support (TRO Support 7)

It is a very good idea for yellow lines on this junction if you live in Holmside or Rosemary Ave you will know how much safer it will make that junction.

Also the junction of Holmside/Rosemary needs to have yellows as you can not see in to Rosemary from Holmside because of parked cars right on the junction.

Third Indication of Support (TRO Support 8)

Please find this email as support in favour on the yellow lines in the Holmside Ave / Queenborough Road Halfway as a resident of Holmside Ave, they are needed to keep this junction clear of parked vehicles.

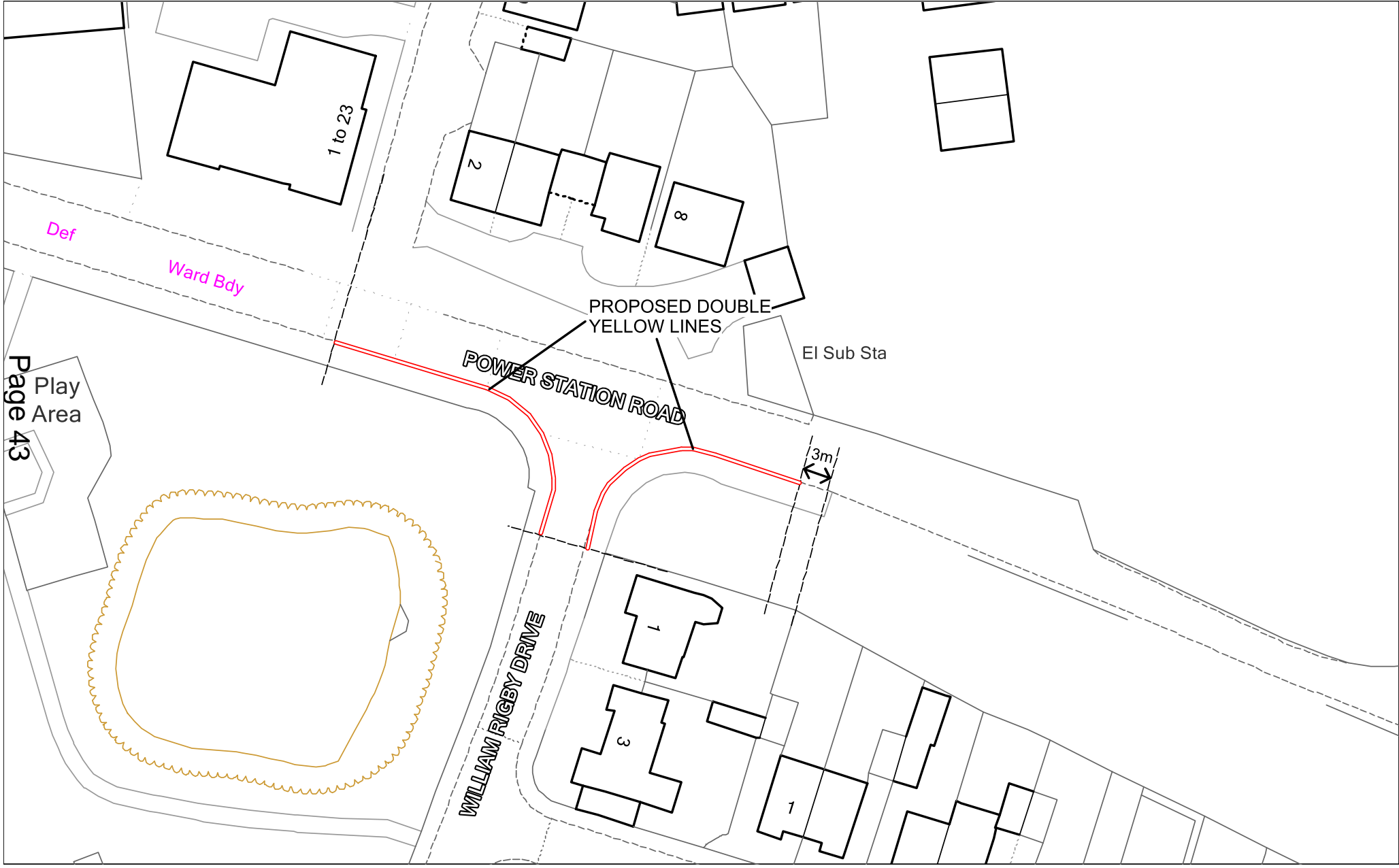
Fourth Indication of Support (TRO Support 4)

I write in support of these yellow lines.. We need them desperately. Somedays You have to wait for traffic to clear the junction before you can attempt to turn in OR out, quite often holding up the traffic flow in Queenboruogh Road in both directions because of dangerous parking on the corners of the junction, and on days when there is a football match, it's even worse. Many of us have given up trying to drive anywhere on those afternoons!

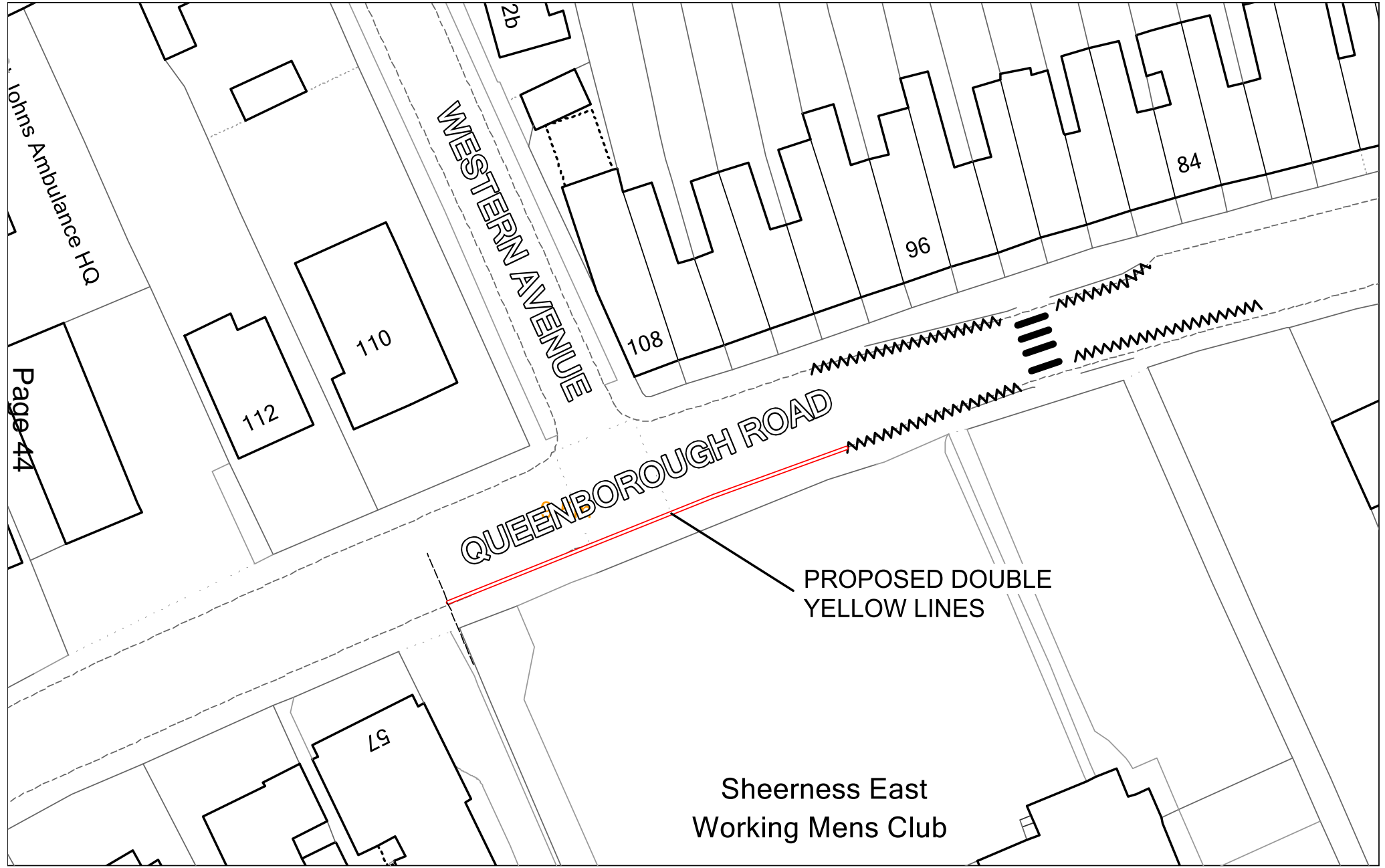
The sooner these yellow lines are painted the better... perhaps they could go right up to Rosemary Avenue corner as well as that too is another bottleneck.

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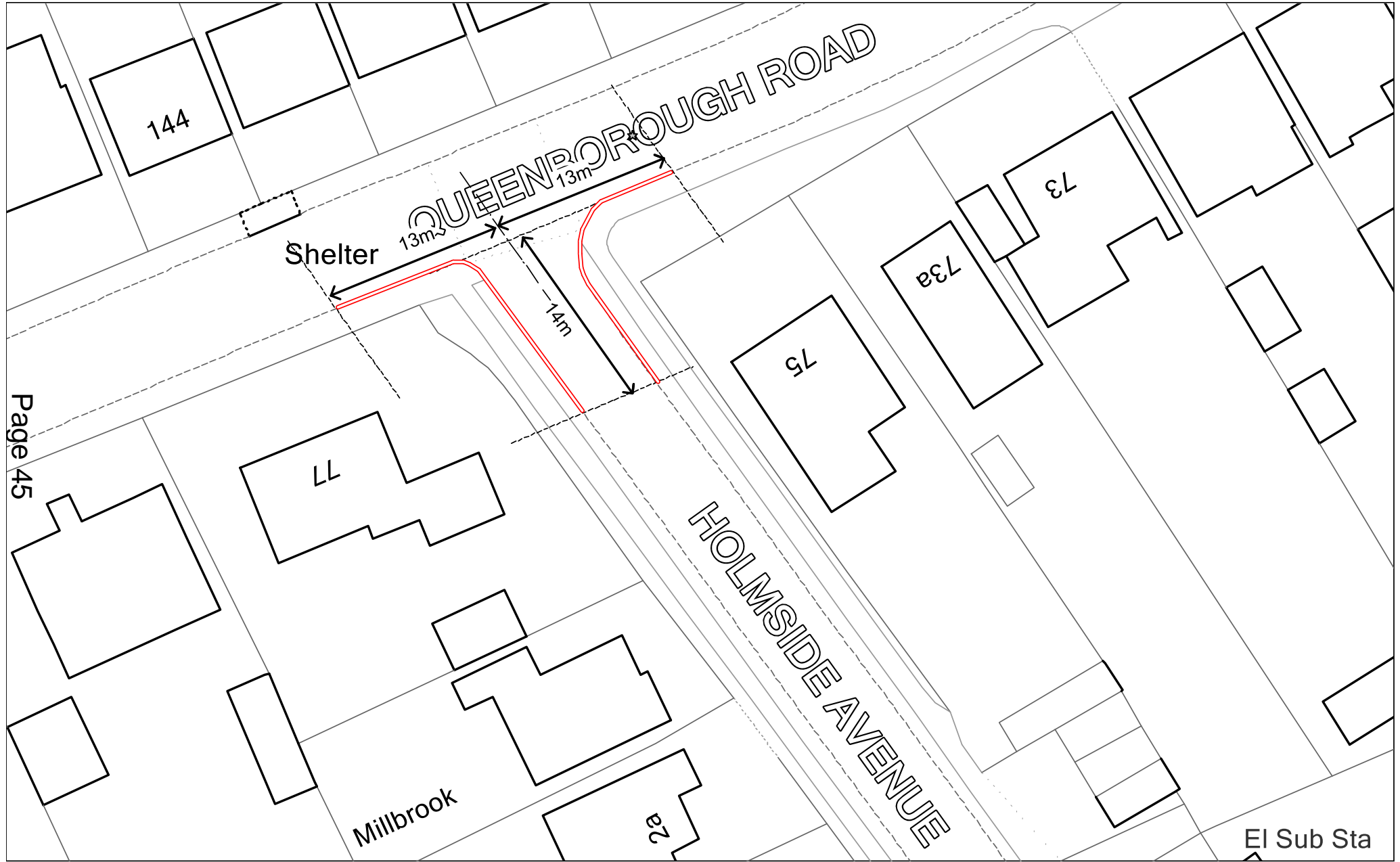
(1) Proposed Double Yellow Lines – Power Station Road/William Rigby Drive, Halfway



(2) Proposed Double Yellow Lines – Sheerness Working Men’s Club, Queenborough Road, Halfway



(3) Proposed Double Yellow Lines – Queenborough Road/Holmside Avenue, Halfway



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SWALE JOINT TRANSPORTATION BOARD	Agenda Item: 8
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Meeting Date	Monday 20 th June 2022
Report Title	Informal Consultation Results – Proposed Double Yellow Lines – Bramley Avenue, Faversham
Head of Service	Martyn Cassell, Head of Commissioning, Environment and Leisure
Lead Officer	Mike Knowles (SBC)
Classification	Open

Recommendations	Members are asked to note the results of the recent informal consultation and recommend that the proposed double yellow lines in Bramley Avenue, Faversham, be progressed through the drafting of a Traffic Regulation Order.
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1. Purpose of Report and Executive Summary

- 1.1 This report provides details of a recent informal consultation undertaken on the possible installation of double yellow lines in Bramley Avenue, Faversham. The requests for additional parking restrictions have come via the County Member and Ward Members, to be funded through the County Members’ Highway Grant.

2. Background

- 2.1 A copy of the informal consultation material sent to residents, including a plan of the proposals, can be found in Annex A. The responses to the informal consultation can be found in Annex B.

3. Issue for Decision

- 3.1 A request was received from the Ward Member for a section of double yellow lines to be installed on the north side of Bramley Avenue in Faversham, near the corner of Preston Avenue. This followed reports of vehicles parking opposite the existing double yellow lines on the south side of the road, forcing vehicles into the centre of the road and creating a conflict with vehicles travelling in the opposite direction.

- 3.2 Proposals were therefore drafted to install a section of double yellow lines to provide a “passing place” for vehicles, whilst maintaining some on-street parking to minimise the impact on parking capacity and regulate traffic speeds.
- 3.3 An informal consultation took place with residents in the vicinity, and the results of this consultation can be found in Annex B. Of the 9 consultation leaflets sent out, 7 responses (78%) were received. Of those 7 responses, 3 supported the proposals, 2 objected and 2 provided comments both in support and objection to the proposals.
- 3.4 Ward, County Member & Town Council Comments. A Ward Member for the area has supported the progression of the proposals, stating *“following a review of the responses I believe that the road safety aspect of installing double yellow lines far outweighs any inconvenience of displaced parking. Road safety must be prioritised over convenience.”* The County Member has confirmed that he is happy to support the views of the Borough Councillors.

4. Recommendation

- 4.1 Members are asked to note the results of the recent informal consultation and recommend that the proposed double yellow lines in Bramley Avenue, Faversham, be progressed through the drafting of a Traffic Regulation Order.

5. Implications

Issue	Implications
Corporate Plan	Improving Community Safety through safer Highways.
Financial, Resource and Property	Cost of Drafting and Advertising Traffic Regulation Order, Cost of Installing Lines and Signs on site. Traffic Regulation Order costs will be funded through County Members Highway Grant.
Legal and Statutory	Advertising of Traffic Regulation Order, reports back to JTB with formal objections, and Sealing by Kent County Council.
Crime and Disorder	None at this stage.
Risk Management and Health and Safety	None identified at this stage.
Equality and Diversity	None identified at this stage.
Sustainability	None identified at this stage.
Health Implications	The installation of double yellow lines at this location should improve vehicle movements and sightlines, resulting in a positive impact on the mental health of those drivers in the area by

	<p>reducing stress levels and potential incidents of road rage.</p> <p>There may be some negative mental health impacts on the owners of the vehicles currently parking in this area as they will need to find an alternative location to park their vehicles.</p>
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6. Appendices

- 6.1 Annex A – Copy of Consultation Material and Plan of Proposals
- Annex B – Results of Informal Consultation

7. Background Papers

- 7.1 None

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IMPORTANT – NOT A CIRCULAR

Proposed Double Yellow Lines Bramley Avenue, Faversham

Following a request from the Ward Member and County Member, proposals have been prepared for an additional section of double yellow lines to be installed in Bramley Avenue, Faversham, as shown on the plan overleaf.

Issues have been reported with vehicles parking opposite the existing double yellow lines on the corner near Preston Avenue, forcing vehicles into the centre of the road and creating a conflict with vehicles moving in the opposite direction. The proposals, funded through the County Member's Highway Grant, have been prepared to provide a passing place for vehicles whilst still allowing some on-street parking at this location to regulate traffic speeds and minimise the impact on on-street parking capacity.

We would be most grateful to receive your views as to whether you support or object to the proposals, and the responses received will be reported to the Swale Joint Transportation Board to consider at their next meeting. Please note that direct, individual responses will not be sent out in response to each questionnaire. At the end of the consultation a report on feedback will be compiled and this will be available on request.

Please e-mail your comments to us at engineers@swale.gov.uk or alternatively complete the reply slip below and return to Swale Borough Council Engineering Services, Swale House, East Street, Sittingbourne, Kent, ME10 3HT, by **Friday 6th May 2022**. A space has also been provided to allow you to add any further comments you may have. Thank you for taking the time to respond.

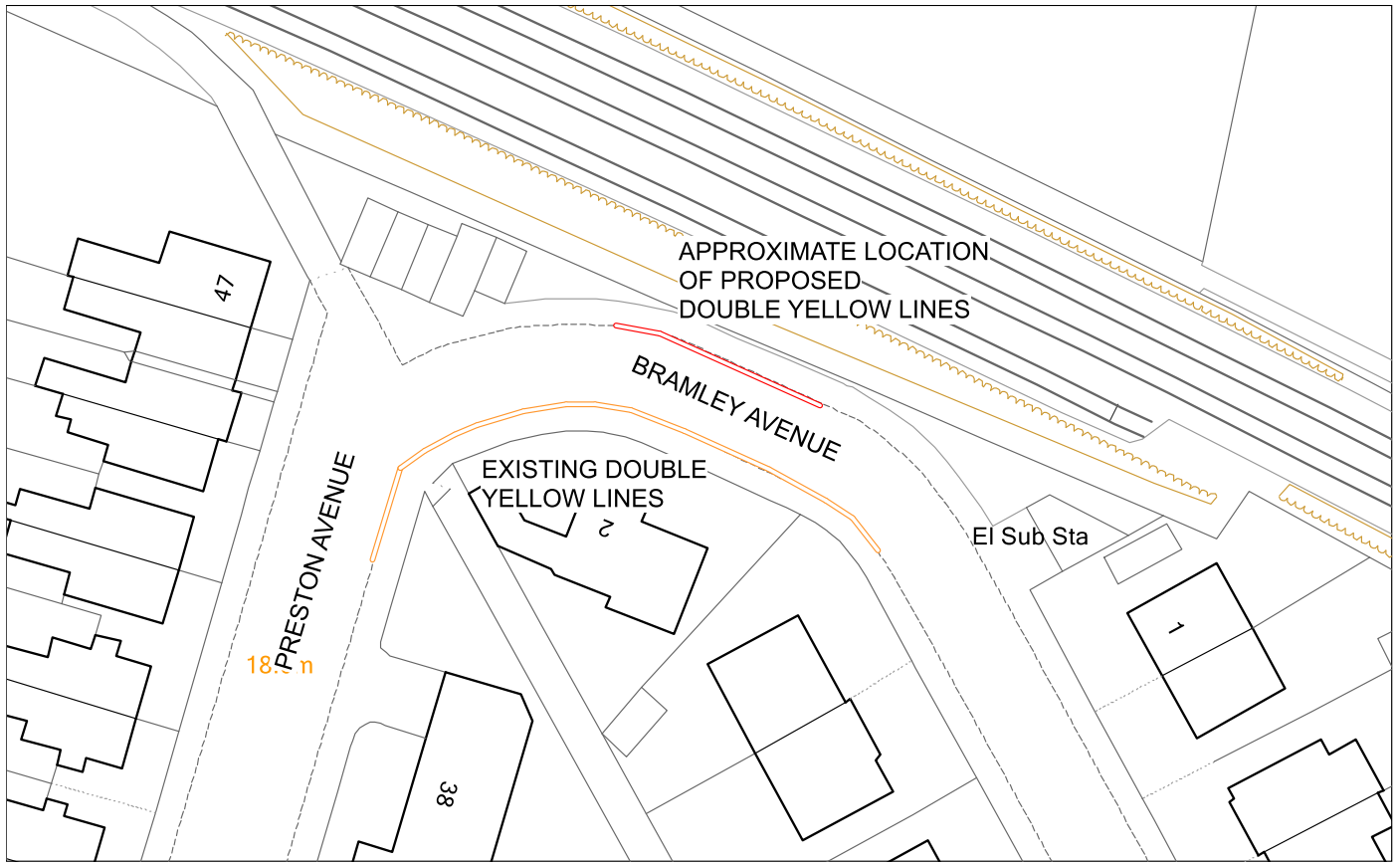
Proposed Double Yellow Lines – Bramley Avenue, Faversham

Please tick one of the following boxes

I Support the proposed double yellow lines I Object to the proposal

Name & Address	Comments

Plan of Proposed Double Yellow Lines – Preston Avenue, Faversham



Proposed Double Yellow Lines - Bramley Avenue, Faversham

Response	Support	Object	Mixed	Comments
1			1	I totally agree with the idea as it causes great problems to traffic coming around the corner both ways. This is mainly to do with the speed they drive down and up. Plus the parking opposite my drive. I live at number * Bramley Avenue and I find it very hard to get off and on my drive as cars always park opposite to my drive. This needs to double yellows up to the sub station. Otherwise there will be more parking opposite my drive all the time. Most of the residents that come down drive far too fast . There will be a bad accident one day, Plus if you do not put the lines further up it will cause a problem for me at all times. When i am driving into my driveway people are unbelievable i e still do not stop to let me in. I e they drive up the pavement rather than stop. Please look at this again as its a problem.
2	1			We support the double yellow line proposal in Bramley Avenue.
3		1		Thank you for your letter regarding the proposed yellow lines on Bramley Avenue, Faversham. Please take this email as my objection to the proposal. My main objection is the yellow lines will significantly restrict parking options near to my house (** Preston Avenue). We park in this area as its the closest to my house. The yellow lines will result in cars being parked significant distance away from homes. This will cause problems for the car owners and also other house holds on Preston Avenue and Bramley Avenue who will not want cars parked outside their house. Furthermore this area is where many rail Engineers park at night to do there work. If they can't park there they will be inconvenienced plus they will have to park outside homes and cause further disturbance. Many thanks for considering my objection to the yellow lines.

Response	Support	Object	Mixed	Comments
4	1			Should make the corner safer if correctly placed, perhaps more left.
5			1	I am in support of the double yellow lines on the corner of Bramley Avenue. This however will not stop cars speeding round that bend and cutting the corner which is a big problem. Also where will be the cars park? It will just push the problem somewhere else up
6		1		Cars parked where proposed lines are slow the traffic down. (not everyone keeps to 20 mph). During my 38 years at ** 2 vehicles have smashed into the block of 5 garages due to excessive speed on the corner.
7	1			It has been a long time coming! Lots of near misses.
Total	3	2	2	

		9	Properties Consulted
% Returned	78	7	No. Returned
% Support	43	3	No. Support Option 1
% Object	29	2	No. Object
% Object	29	2	No. Both Supporting & Objecting

PROPOSED TRAFFIC CALMING, TONGE ROAD, SITTINGBOURNE

To: **Swale Joint Transportation Board – 20 June 2022**

By: **Tim Read, Head of Transportation, Kent County Council**

Classification: **Unrestricted**

Ward: **Murston, Swale District**

Summary: **This report gives details of a proposed traffic calming scheme on Tonge Road (between Church Road and All Saint Road) and the results of the statutory consultation.**

For Recommendation

1.0 Introduction and Background

- 1.1 From November 2019 Kent County Council discussed with the local Councillor for the area proposals to provide engineering measures to encourage slower driver behaviour along the 30mph section of Tonge Road, Sittingbourne. This was initially driven as a Combined Member Grant (CMG) Scheme by KCC member Jason Clinch and District Councillor James Hall. Outline scheme drawings were derived along with traffic surveys collected and analysed to assess the feasibility of traffic calming. The main driver for the scheme was to achieve lower driven speeds along Tonge Road and included improvements to the side junctions of Oak Road and Portland Avenue.
- 1.2 Due to insufficient CMG funding, a bid to use Local Transport Plan (LTP) funding was secured.
- 1.3 Due to the Covid 19 Pandemic, works were halted and LTP funding for the scheme was carried over to 2021 where further feasibility studies were carried out including topography surveys and road safety audits. Throughout the scheme, changes have been made and the scheme has been amended to suit the geometry of the road and the existing features along Tonge Road such as drainage, bus stop locations and existing parking arrangements. James Hall carried out various informal consultation exercises within the local neighbourhood to gain the general consensus for calming measures to which it was reported back to KCC that this was on the whole a much-needed scheme with overall support.
- 1.4 At present, Tonge Road is predominantly a 9m wide single carriageway route that connects Church Road, Tonge Corner (east) to Church Road (west). The speed limit is 30mph by way of a series of streetlights in a built-up environment. There is a speed limit change to national 30m east of All Saints

- Road. The existing road layout is very linear and does not encourage drivers to slow down. Instead motorists can maintain speeds through the existing road geometry.
- 1.5 Murston School is located on Church Road, 70m north of the junction with Tonge Road.
 - 1.6 A total of six personal injury collisions resulting in 3 slights, 2 serious and a fatal collision (50% KSI) in the three-year period ending 31st December 2020. The fatal collision involved a motorcyclist and a pedestrian. The two serious collisions were to pedestrians.
 - 2.0 Scheme proposals (see Appendix A for details)–
The proposal introduces the following features to reduce traffic speeds on Tonge Road between All Saints Road and Church Road.
 - 4 no. speed cushions located between All Saints Road and Portland Avenue.
 - New Raised Tables at junctions with Church Road, Portland Avenue, Oak Road and All Saints Road
 - Removal of existing raised hump on Church Road
 - Introduction of new 24m length double yellow line restriction on Church Road and
 - Introduction of new 25m length double yellow line restriction on Tonge Road.
 - 3.0 **The Consultation**
 - 3.1 The consultation took place from Friday 22 April until Monday 16 May 2022.
 - 3.2 Numerous residential properties are affected directly by the proposal.
 - 3.3 The consultation material was placed on the highway hub of KCC's consultation website '[Let's talk Kent](#)' and sent to statutory consultees and local residents by post and email. A number of public notices were placed on site and an advert was placed in the local paper. An email was sent to 741 people who had asked to be kept informed of KCC consultations regarding transport and roads in the district of Swale. Hard copies of the consultation material were made available upon request.
 - 3.4 The changes to waiting restrictions require a Traffic Regulation Order (TRO) and statutory consultation period of 21 days. We also wanted to provide the opportunity for residents and other stakeholders to provide feedback on the rest of the proposed scheme, so we expanded the remit of the TRO consultation.
 - 3.5 In addition to the TRO paperwork, a consultation document, full scheme plan and Equality Impact Assessment were produced to provide information on the whole scheme.
 - 3.6 The consultation questionnaire was in two parts. Part one for the mandatory questions for the TRO. In this section consultees were asked if they supported

or opposed the waiting restriction amendments and asked to give their reason(s). Part two contained optional questions to help us better understanding of their views on the scheme as a whole.

- 3.7 26 responses were received for the Traffic Order Regulations changes, 16 objected and 10 supported. 17 responses were received for the Traffic calming changes, eight objected and nine supported.
- 3.8 Respondents were asked to provide the reason for their response. These have been grouped into themes. The main theme of those supporting the scheme include reduced speed in residential areas is a good objective and speed cushions will reduce rat running. The main theme of those opposed to the scheme included loss of parking spaces and the belief that speed cushions will not reduce speeds. See Appendix B for full consultation report.
- 3.9 Key comments for JTB to consider:
- Traffic calming measures are considered a positive addition to the scheme.
 - The current parking issues may be alleviated by adding more parking options at the electrical distribution plant location however this is not being promoted or progressed by KCC.
 - The loss of six parking bays is necessary to ensure the raised table is clear of visual obstruction.

4.0 Corporate Implications

4.1 None for Swale Borough Council

5.0 Financial

5.1 None for Swale Borough Council.

6.0 Legal

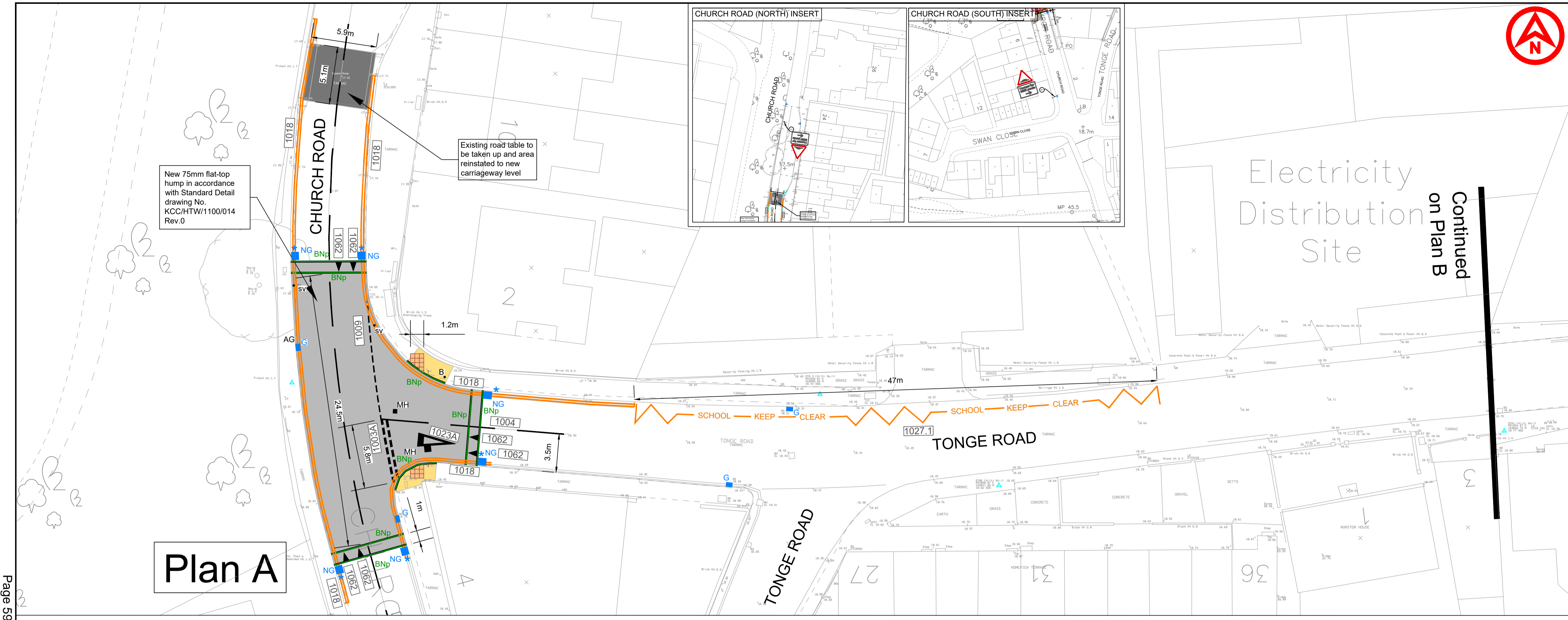
6.1 None for Swale Borough Council.

7.0 Recommendation(s)

7.1 That members agree to the implementation of the proposed traffic calming measures.

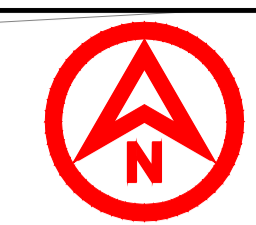
Contact Officer:	Alan Osuoha, Schemes Project Manager, Kent County Council, 03000 418181
Reporting to:	Tim Read, Head of Transportation, Kent County Council, 03000 418181

Appendix A – Plan of scheme



Plan A

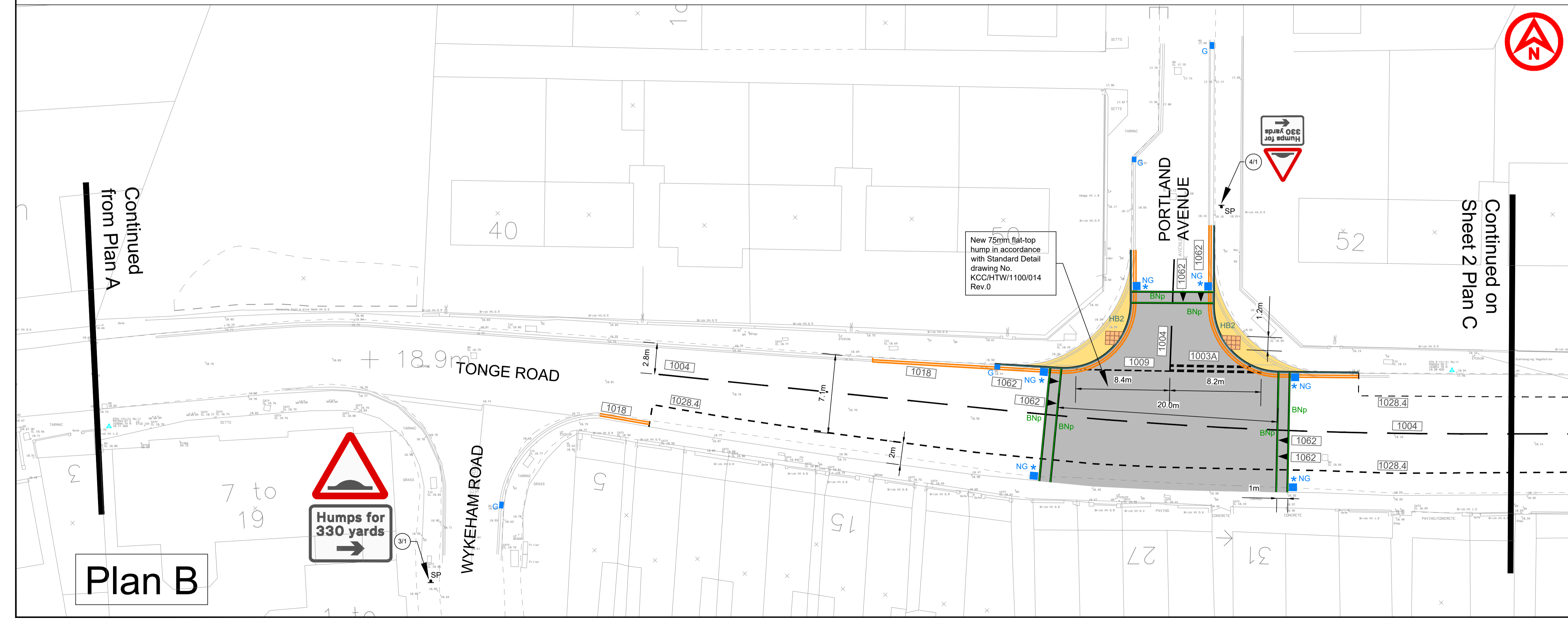
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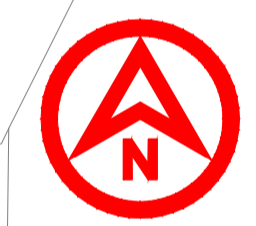
- Notes:**
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 - All setting out is to be agreed on site with the Engineer prior to commencement of works.
 - All precast concrete (PCC) kerbing and edging shall be in accordance with BS7263, Part F.
 - Any voids between new kerbs and existing carriageway shall be reinstated with 10mm AC 10 close surf.
 - Surfaces between bituminous materials shall be saw cut to provide a clean vertical joint.
 - Vertical faces of existing bituminous materials shall be primed with bitumen. Existing horizontal or inclined surfaces shall be tack coated in accordance with Appendix 7.4.
 - New footway surfaces shall be laid with 1:40 crossfall towards carriageway unless otherwise stated with no point having a crossfall greater than 1:12.
 - Existing surfaces overlaid shall be swept clean prior to overlay and sprayed with tack coat.
 - All existing covers in areas to construction to be raised to suit new levels.
 - All existing road markings that conflict with the proposed are to be removed.

- General key**
- Existing lamp column to remain.
 - Existing vehicle crossover to remain.
- Kerbing / Footway / Carriageway Construction Key**
- Overlay footway with AC6 dense surf 100/150 20mm thick over AC10 regulating layer. Perimeter of surface course to be keyed into existing surface course to give a minimum layer thickness of 20mm. All joints to be saw cut.
 - New raised table: 40mm SMA10 surface course 40/60 WTR1 laid on 60mm HRA 60/20 F bin 40/60 des WTR1
 - Reinstatement for carriageway: 40mm SMA10 surface course 40/60 WTR1
 - HB2 Proposed 125 x 255mm half battered kerbing (Type HB2 or SP). *Haunch/Variation A / B* upstand 125mm to KCC/1100/001/D.
 - BNp Proposed 125 x 150mm bullnose kerbing (Type BN). Upstand 0-6mm to KCC/1100/001/D.
 - Buff PCC tactile paving 400 x 400mm: 50mm thick slabs, to KCC/1100/015. Blisters to align with tactile on opposite side of road.
 - NG New PCC trapped gully with class D400 grate as KCC/SD/500/009.
 - Connection of 1500 pipe to new PCC chamber.
 - Existing road gully
 - MH Adjust existing manhole cover and frame to new road level
 - SW Adjust service covers to new road level
 - AG Adjust gully covers to new road level

- Sign & Lines Key**
- Non-illuminated traffic sign (refer to Sign Schedule).
 - Sign position and orientation.
 - New / Refreshed Road marking to TSRGD 2016 Diagram number.
 - White Road marking.
 - Yellow Road marking.
 - Remove from set aside and reinstall bollard to new location.



Plan B



Rev	Revision Date	Purpose of revision	Drawn	Check'd	App'd
2	17/09/2021	Draft issued for RSA Stage 2			
1	01/06/2021	Draft Addressed issues raised in RSA1			
0	04/05/2021	Draft First Issue			

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 Kent County Council
 Ashford Highway Depot
 Herwood Industrial Estate
 Ashford
 TN24 8AD
 Tel: 03000 418181

Project		Schemes Planning & Delivery	
Drawing title		Tongue Road, Sittingbourne Traffic Calming scheme General Arrangement - Sheet 1 of 3	
Drawing status		DRAFT	
Scale	1:200 @ A1	Do not scale	
Drawing number	20/21-CMG-SW-1248-GA	Rev	2

This drawing is not to be used in whole or part other than for the intended purpose and project as defined on this drawing. Refer to the contract for full terms and conditions.



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 - New raised table:
 - 40mm SMA10 surface course 40/60 WTR1 laid on,
 - 60mm HRA 60/20 F bin 40/60 des WTR1
 - Reinstatement for carriageway:
 - 40mm SMA10 surface course 40/60 WTR1
 - Proposed 125 x 255mm half battered kerbing (Type HB2 or SP). *Haunch/Variation A / B* upstand 125mm to KCC/1100/01/D.
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 - Buff PCC tactile paving 400 x 400mm. 50mm thick slabs, to KCC/1100/015. Blisters to align with tactile on opposite side of road.
 - New PCC trapped gully with class D400 grate as KCC/SD/500/009.
 - Connection of 1500mm pipe to new PCC chamber.
 - Existing road gully
 - Adjust existing manhole cover and frame to new road level
 - Adjust service covers to new road level
 - Adjust gully covers to new road level

- Sign & Lines Key**
- Non-illuminated traffic sign (refer to Sign Schedule).
 - Sign position and orientation.
 - New / Refreshed Road marking to TSRGD 2016 Diagram number.
 - White Road marking.
 - Yellow Road marking.
 - * Remove from set aside and reinstall bollard to new location.

Continued from
Sheet 1 Plan B

Continued
on Plan D

Plan C

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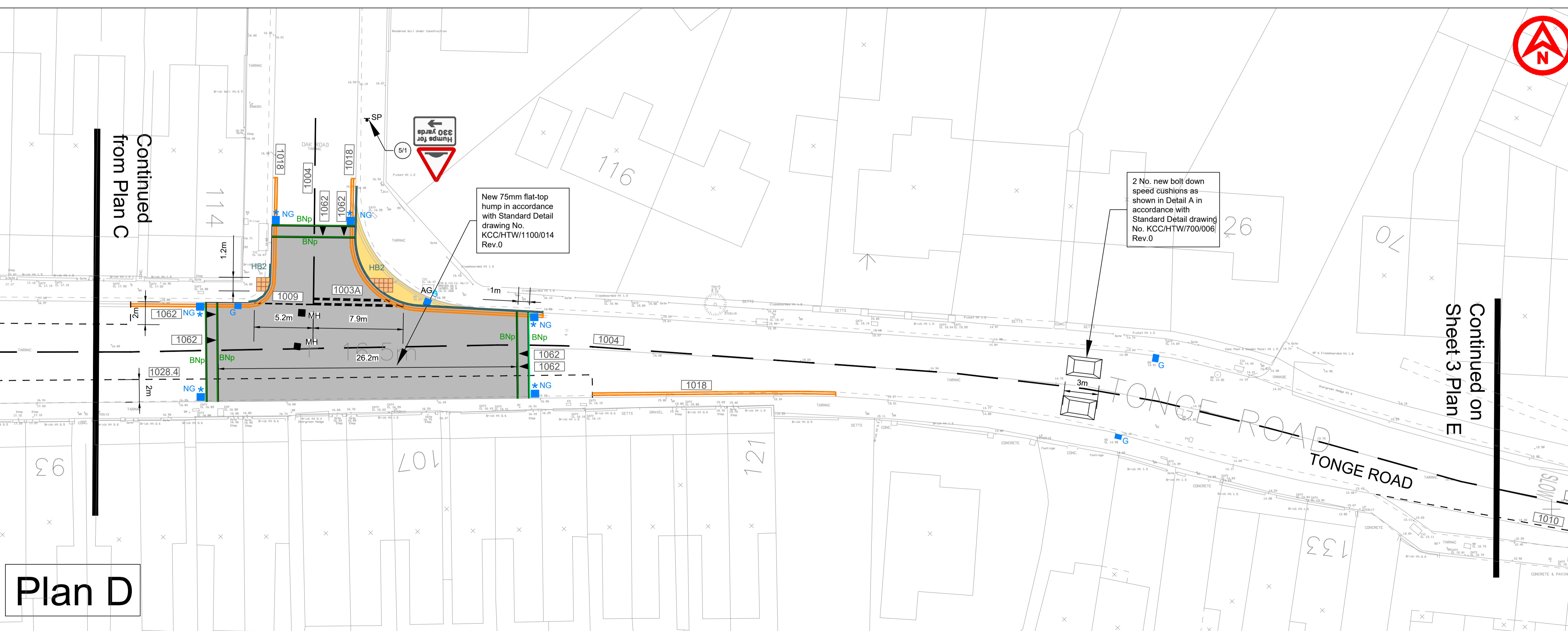
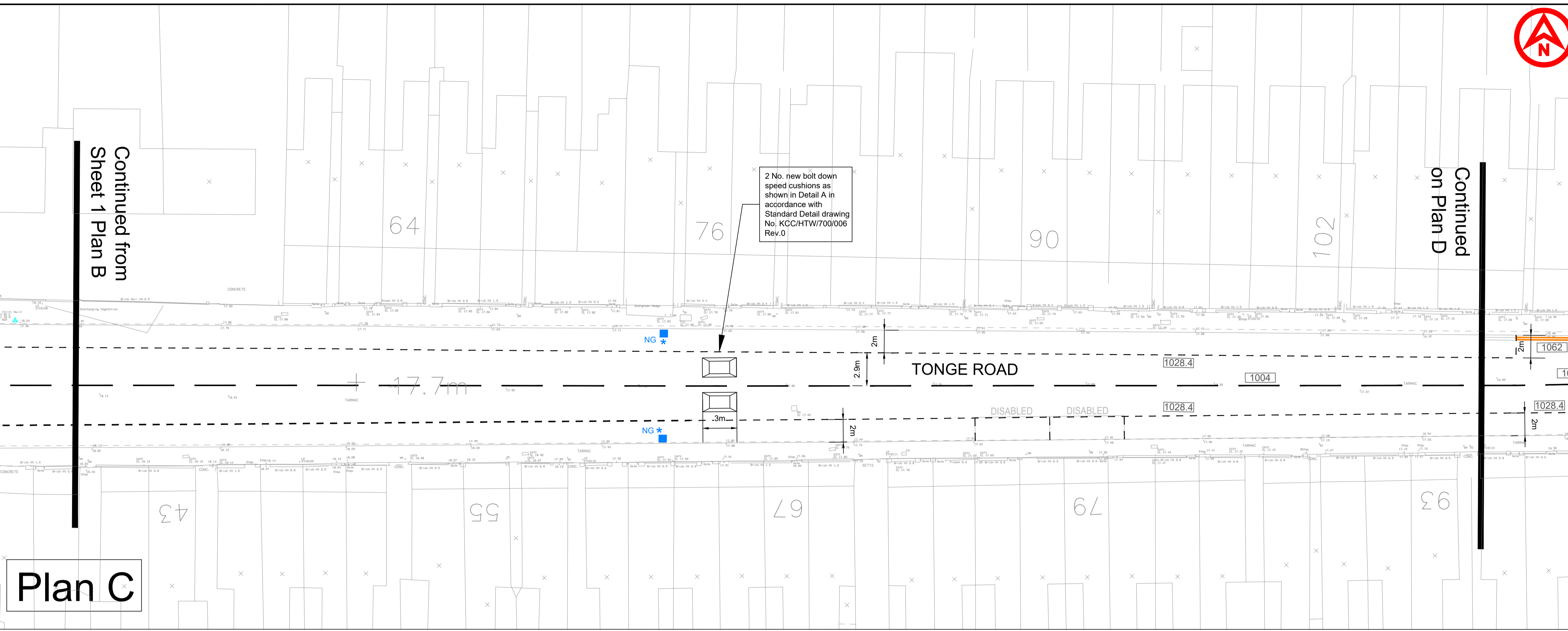
2	17/09/2021	Draft issued for RSA Stage 2	CLS	
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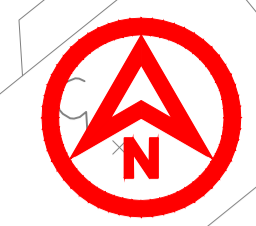
Project		Schemes Planning & Delivery	
Drawing title		Tonge Road, Sittingbourne Traffic Calming scheme General Arrangement - Sheet 2 of 3	
Drawing status		DRAFT	
Scale		1:200 @ A1	Do not scale
Drawing number		20/21-CMG-SW-1248-GA	Rev 2

This drawing is not to be used in whole or part other than for the intended purpose and project as defined on this drawing. Refer to the contract for full terms and conditions.



Plan D

Continued on
Sheet 3 Plan E



- Notes**
1. Do not scale from this drawing.
 2. All dimensions are proposed and in metres unless otherwise stated.
 3. All setting out is to be agreed on site with the Engineer prior to commencement of works.
 4. All precast concrete (PCC) kerbing and edging shall be in accordance with BS7263, Part F.
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 11. All existing road markings that conflict with the proposed are to be removed.

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Continued from Sheet 2 Plan D

New 75mm flat-top hump in accordance with Standard Detail drawing No. KCC/HTW/1100/014 Rev.0



Plan E

2	17/09/2021	Draft issued for RSA Stage 2	CLS	
1	01/06/2021	Draft Addressed issues raised in RSA1	JOG	
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Rev	Revision Date	Purpose of revision	Drawn	Chac'd App'd

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Project		Schemes Planning & Delivery	
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Drawing status		DRAFT	
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Drawing number	20/21-CMG-SW-1248-GA	Rev	2
This drawing is not to be used in whole or part other than for the intended purpose and project as defined on this drawing. Refer to the contract for full terms and conditions.			

Appendix B – Consultation responses

Church Road and Tonge Road, Sittingbourne Traffic Calming Scheme

Church Road and Tonge Road, Sittingbourne Consultation Report

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May 2022

Alternative Formats: This document can be made available in other formats or languages, please email alternativeformats@kent.gov.uk or telephone 03000 421553 (text relay service 18001 03000 421553). This number goes to an answer machine, which is monitored during office hours.

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Church Road and Tonge Road, Sittingbourne Traffic Calming Scheme



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1. Introduction

1.1 Introduction

From 22 April to 16 May 2022, Kent County Council (KCC) consulted on proposals for highway improvements on Church Road and Tonge Road, Sittingbourne in the district of Swale. The following report details the purpose of the consultation and summarises the consultation process and feedback received.

1.2 Purpose of the Consultation

KCC is proposing to introduce traffic calming measures on Church Road and Tonge Road in Sittingbourne, to reduce vehicle speeds and therefore the risk of serious road traffic accidents on these stretches of road. The scheme includes changes to waiting restrictions, which require a Traffic Regulation Order (TRO). The effect of this proposed Order would be to introduce new and extend or amend existing waiting restrictions (double yellow lines and parking bays) on Church Road and Tonge Road. Full details can be found in the Public Notice and Statement of Reason document on the consultation webpage www.kent.gov.uk/tongeroadtrafficalming. TROs are subject to a statutory consultation period before a decision can be made to make the Order permanent.

We also wanted to give residents and other stakeholders the opportunity to provide feedback on the rest of the proposed scheme. A consultation document and scheme plan for the whole scheme was also available from the consultation webpage.

2 Consultation Process

This chapter outlines the process followed to deliver the consultation and details the activities and documentation developed to support the delivery of the consultation. The consultation was divided into the five stages. Detailed information on each section is given in this Chapter.

Undertake Equality Impact Assessment (see Chapter 3)	Develop consultation process and promotional activities	Pre-consultation activity/engagement	During consultation activity	Post consultation activity
<ul style="list-style-type: none"> Identify possible impacts on protected characteristic groups 	<ul style="list-style-type: none"> Identify stakeholders Define consultation activities Define communication activities and frequencies 	<ul style="list-style-type: none"> Meeting with Swale Borough Council and Local KCC Member 	<ul style="list-style-type: none"> Consultation documents delivered to businesses and residents in and around Tonge Road Public Notices displayed on lamp posts Launched consultation webpage and questionnaire Advert in local newspaper Responded to queries 	<ul style="list-style-type: none"> Analysis and reporting of consultation responses Feedback to consultees and stakeholders Attendance at Swale Joint Transportation Board

2.1 Pre-Consultation Activities

KCC officers held meetings with the local Swale District Council councillor to help identify local concerns, aspiration, and site limitations. The information from these meetings informed the design of the proposed scheme. These meetings also identified issues beyond the remit of this project. This information will be feedback to the appropriate teams at KCC.

2.2 Promoting the Consultation

The following promotional activities were undertaken to support the delivery of the public consultation:

- Advert in Kent Messenger newspaper on 22 April 2022.
- Public Notice displayed on Tonge Road and Church Road on lamp posts in the proposed scheme vicinity.
- Webpage on [Highways Hub](#) on KCC's Let's talk Kent engagement website – www.kent.gov.uk/tongeroadtrafficalming
- Email invite sent to 741 people who have requested through Let's talk Kent to be kept informed of consultations on transport and roads in the district of Swale.
- Email to statutory stakeholders.
- Consultation document delivered to residents and businesses in the immediate vicinity of the proposed scheme.

2.3 Consultation Material

The following material was produced for the consultation and was made available on the consultation webpage:

- Consultation Document
- Equality Impact Assessment
- TRO Public Notice, including Statement of Reason
- Scheme plans
- Consultation questionnaire

Hard copies of the consultation document were delivered to residents most impacted by the scheme.

In total there were **312** document downloads from the webpage. It was visited **529** times with **497** unique visitors.

2.4 Feedback Mechanism

People were asked to provide feedback via a consultation questionnaire, which was available online and in a Word version. The Word version was available in hard copy on request via telephone or email. Emails and letters were also accepted and analysed alongside responses to the questionnaire.

3 Equality and Accessibility

3.1 Equality Impact Assessment (EqIA)

The EqIA provides a process to help us to understand how the proposals may affect people based on their protected characteristics (age, disability, sex, gender identity, race, religion / belief or none, sexual orientation, pregnancy and maternity, marriage and civil partnership and carer's responsibilities).

An EqIA was completed prior to commencement of the consultation and was available as one of the consultation documents on the webpage and on request. We will use the feedback gathered from the consultation to review and update the EqIA before the detailed design is finalised.

The following steps were taken to help ensure the consultation was accessible:

- In addition to the consultation being available online, hard copies of the consultation document were posted to residents most impacted by the proposals.
- Hard copies of the consultation questionnaire and other material was available on request throughout the consultation period.
- All consultation material included a phone number and email address for people to request hard copies and alternative formats of the consultation material or to ask a question.
- All documents uploaded to the webpage were accessible for people using assisted technology.

In the second part of the consultation questionnaire we asked respondents if there was anything we should consider relating to equality and diversity (Q7). Only three people answered this question. One person said that young and old would benefit from improvements to the road and another that it needs permit only parking with one per house only. The third responded with n/a.

4 Consultation Results

This chapter summarises the number of consultation responses received and who responded to the consultation. There was a total of 26 responses to Part One of the questionnaire and 17 responses to Part Two.

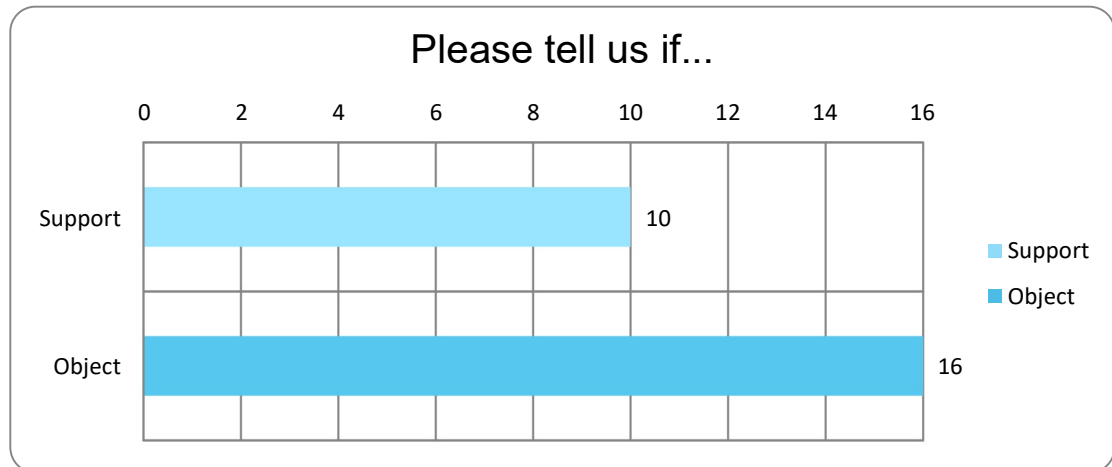
4.1 Part One – Traffic Regulation Order

Part one of the questionnaire asked the mandatory questions for the Traffic Regulation Order.

4.1.1 Q1. Please tell us if you support or object to this Traffic Regulation Order to introduce waiting restrictions at any time on Church Road and Tonge Road in Sittingbourne.

There were 26 responses to this question:

- 10 respondents supported the traffic regulations order
- 16 respondents objected to the traffic regulations order.



4.1.2 Q1a. Please tell us, in the box below, the reason for your support or objection.

There were 26 responses to this question. These responses were grouped into themes as shown in the table below. Some responses had more than one theme so therefore the number of themes will be greater than the number of respondents.

- 10 respondents were in favour of scheme for pedestrians, environment and slowing traffic down.

Church Road and Tonge Road, Sittingbourne

Traffic Calming Scheme

- 16 were not in favour with the proposed traffic calming measures and felt that the introduction of speed camera is a better solution to reduce speeds of traffic

Frequency	Theme / Comment
Support	
5	Reduced speed in residential areas is a good objective
4	Agree with speed cushions
2	Extend proposal to Lower Road or Station Road Teynham
1	Main concern is ensuring the speeding cars HAVE to slow down
1	Traffic calming on this rat-run is welcomed
1	The new arrangements should improve lines of sight at junctions and is deserving of support
1	Pavement parking are an issue so restricting waiting if it is enforced would be a good thing

Church Road and Tonge Road, Sittingbourne

Traffic Calming Scheme

Frequency	Theme / Comment
Objection	
11	Existing parking issues made worse by proposal - residents have to park a distance away already
3	Waste of money - causes more harm than good
2	Existing parking on All Saint Road is at the junction mouth which blocks visibility
2	Raised table design encourages more vehicles to risk head on collision with would-be boy racers
2	Disagree with speed humps
1	Speed humps can cause damage to vehicles
1	Speed cushions layout will be ignored or drivers will stop for some but not for others
1	Parked vehicles keep road safe
1	Feel ignored from the consultation process owing to poor communication – a letter drop is preferred to online options like Twitter
1	Parking problems are already an issue. Residents should be given individual permits

Church Road and Tonge Road, Sittingbourne Traffic Calming Scheme

1	Zebra crossing will make it safer for kids
1	Opportunity to add extra parking by electricity distribution site
1	Full width speed humps should be used instead of cushions on Tonge Road
1	Install cameras

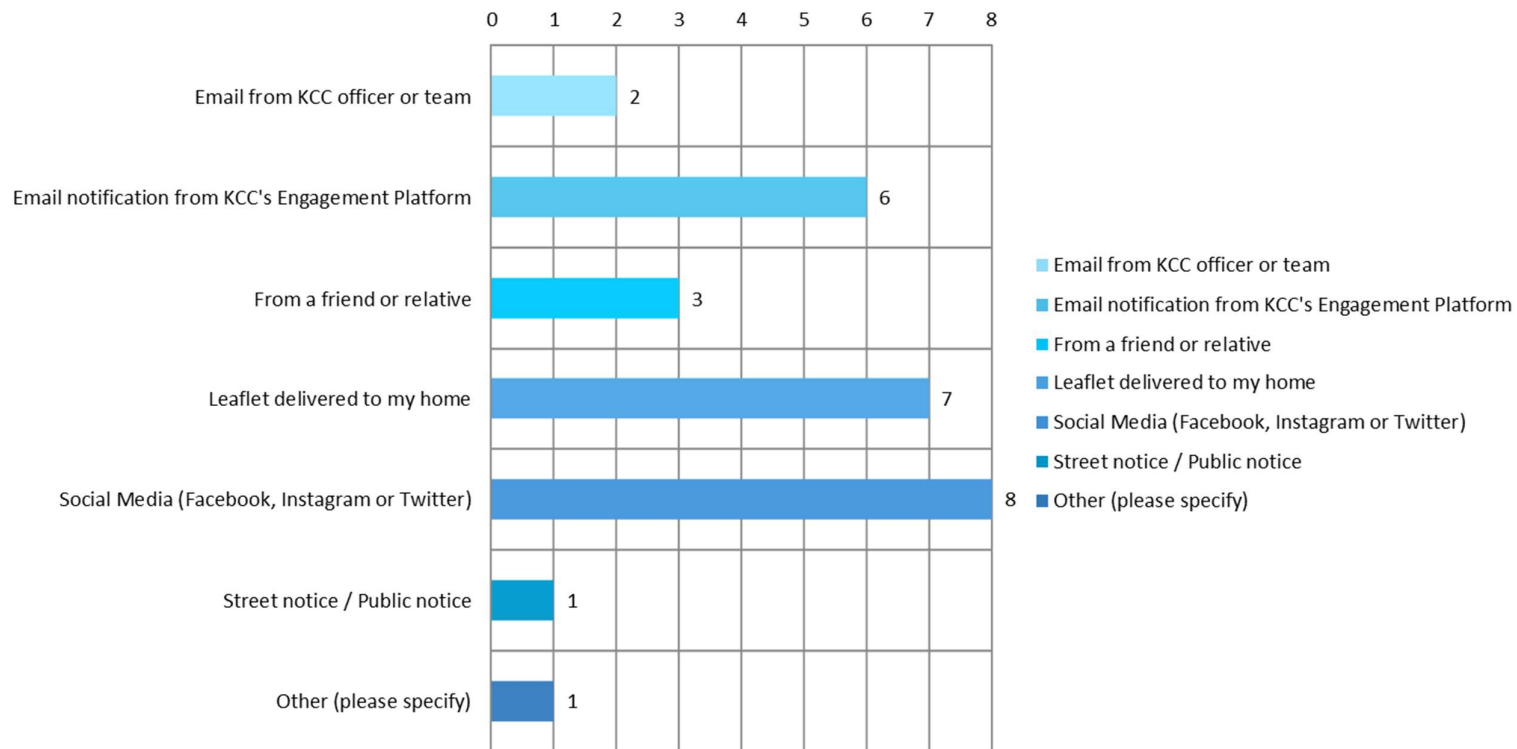
4.1.3 Q2. How did you find out about the consultation?

There were 25 responses to this question. Please note respondents were able to select multiple responses to this question.

- 8 were alerted to this consultation via social media (Facebook or Twitter)
- 7 respondents were informed by leaflet delivered by mail
- 6 by email notification from KCC's Engagement Platform
- 3 from a friend or relative
- 2 from an email from a KCC officer / team
- 1 was informed by street / public notice
- 1 respondent selected 'Other' and left the reason blank.

Church Road and Tonge Road, Sittingbourne Traffic Calming Scheme

How did you find ...



Church Road and Tonge Road, Sittingbourne Traffic Calming Scheme



4.2 Part Two – Church Road and Tonge Road Traffic Calming Scheme

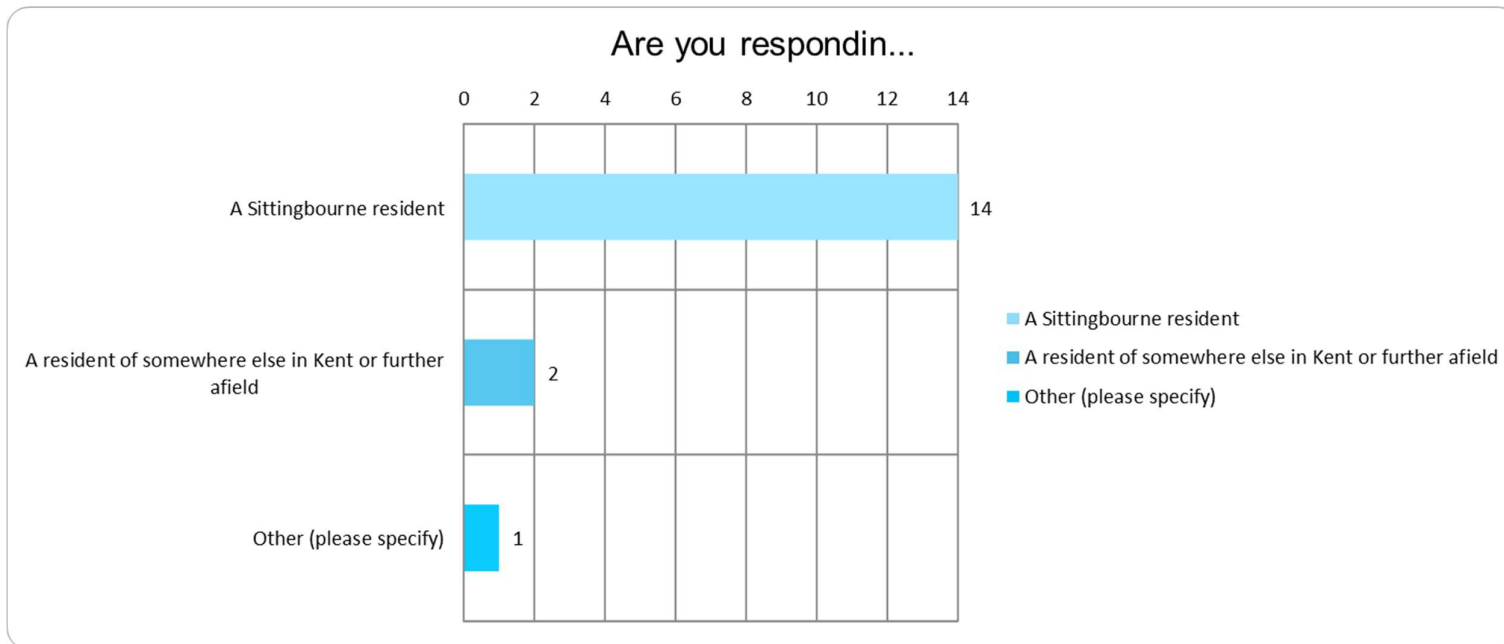
After completing part one respondents were asked if they would answer some optional questions to help us better understand their views on the scheme as a whole. 17 people selected to answer the second part of the questionnaire.

4.2.1 Q3 Are you responding as a...?

Please select the option from the list below that most closely represents how you will be responding to this consultation. Please select one option.

- 14 respondents were residents of Sittingbourne
- 3 were residents outside of Sittingbourne (this includes one respondent who selected 'Other' and specified that they are a 'rural resident of the Borough of Swale').

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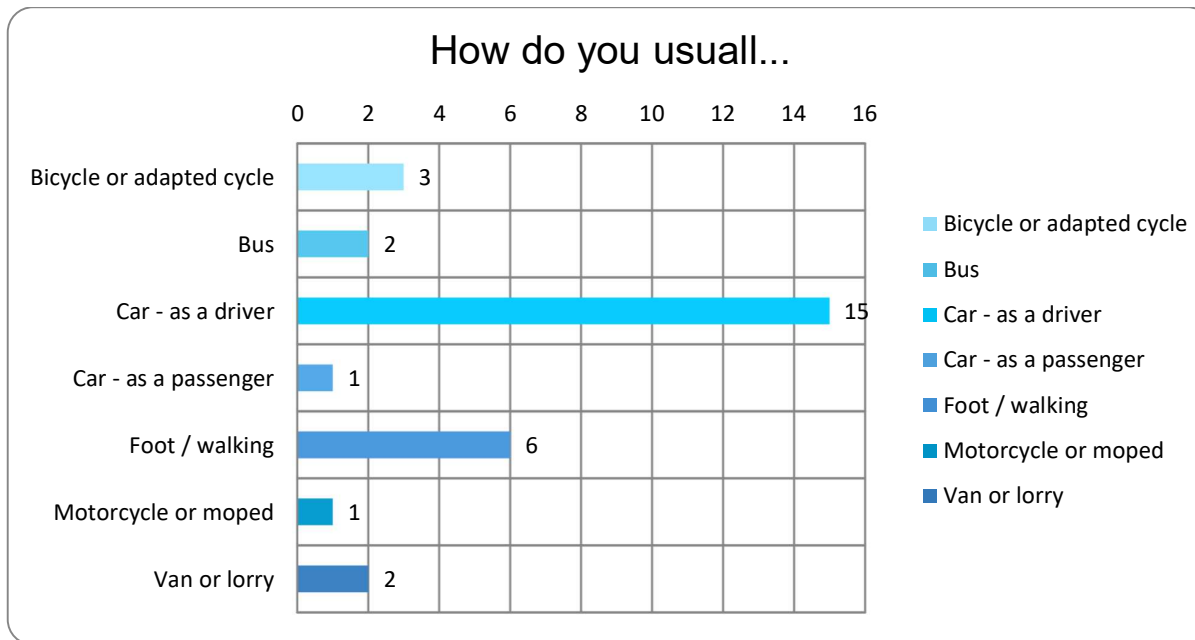


Church Road and Tonge Road, Sittingbourne Traffic Calming Scheme

4.2.2 Q5. How do you usually travel in and around Sittingbourne?

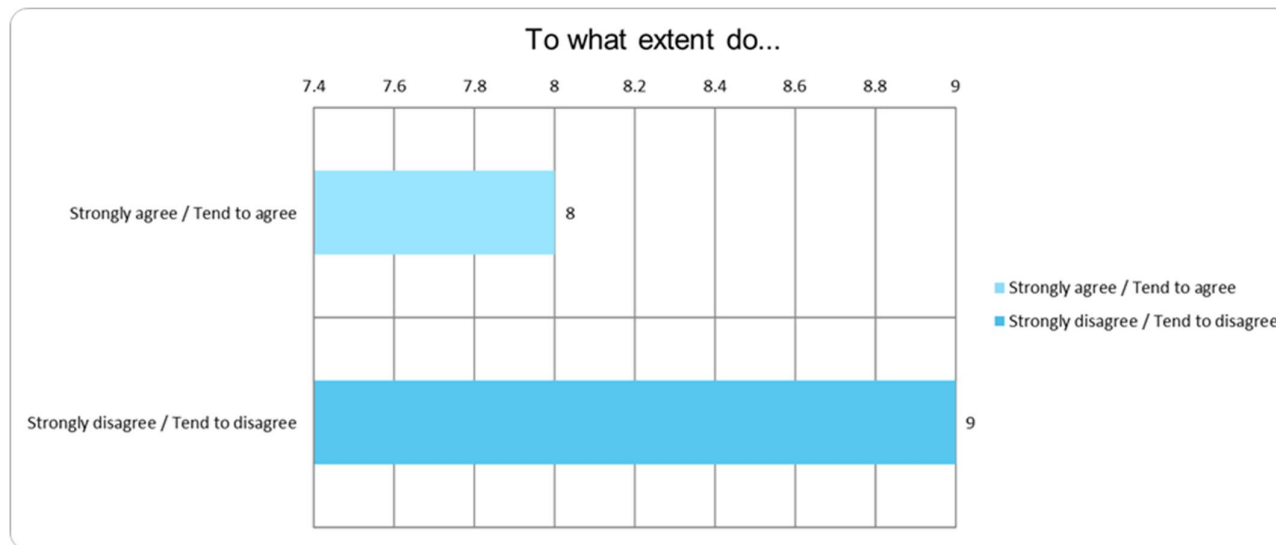
There was a total of 30 responses to this question. Please note respondents were able to select multiple responses to this question

- Most of the respondents are car drivers with a total of 15
- 9 respondents either walk (6) or cycle (3)
- 2 respondents selected bus and 2 selected van or lorry
- 1 respondent selected a car passenger and 1 selected motorcycle or moped



4.2.3 Q6. To what extent do you agree or disagree with the proposed Church Road and Tonge Road traffic calming scheme?

- 8 respondents (5 strongly and 3 tend to) agree with the traffic calming scheme
- 9 respondents (8 strongly and 1 tend to) disagree with the scheme.



4.2.4 Q6b. Please tell us the reason for your answer

There were 16 responses to this question. These responses were grouped into themes. In summary:

- 8 of respondents were in favour of scheme for pedestrians, environment and slowing traffic down

Church Road and Tonge Road, Sittingbourne Traffic Calming Scheme

- 4 not in favour of the scheme, with a reduction of traffic calming and/or speed camera as a better solution to reduce speeds of traffic

A breakdown of the themes in agreement and disagreement are shown in the tables below:

Frequency	Theme / Comment
Support	
8	Traffic calming is good for the area.
4	Church Rd or Tonge Rd is not suitable for rat running traffic
2	Traffic calming will remove rat-running
1	Traffic calming is also required in Tonge Rd, Church Rd & Station Rd Teynham
1	Agree with traffic cushions

Frequency	Theme / Comment
Disagree	
2	Why not limit to a 20mph?
1	Traffic calming on Church Rd does not eliminate rat-running
4	Reducing parking will be detrimental to residents

Church Road and Tonge Road, Sittingbourne

Traffic Calming Scheme

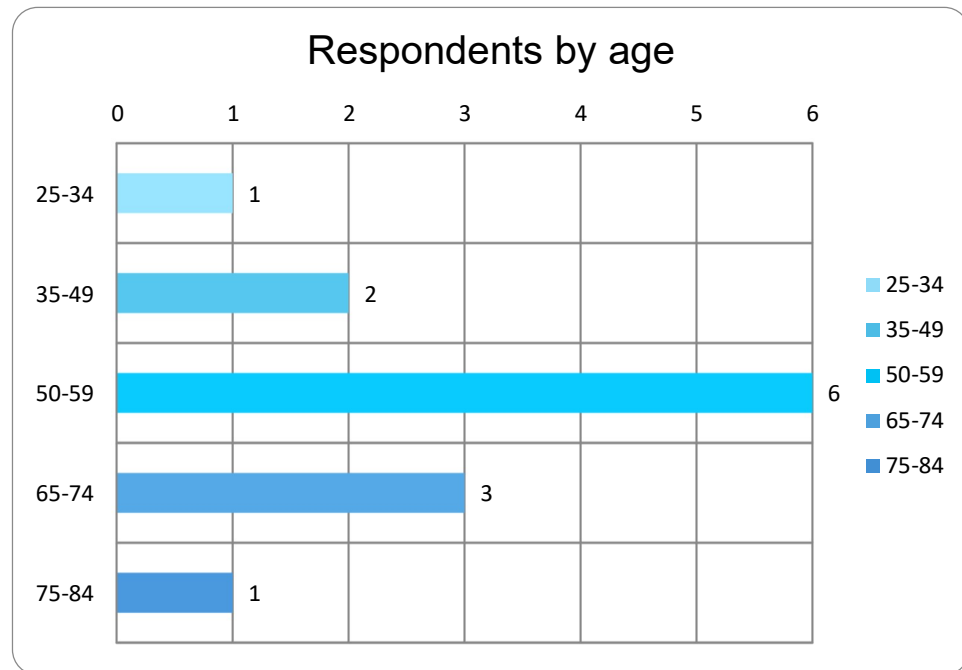
2	Proposal will not reduce rat running
1	Speed cushions not supported
1	Road is not dangerous, and accidents is not an issue
1	Permit parking is required. 1 per house
1	Zebra crossing will be a good thing
1	One accident on Tonge Rd was caused during a large gathering and not linked to the road
2	Traffic along Murston Rd is already congested
1	Disagree with proposed kerb realignment

4.3 Respondent Demographics

The following section documents the demographics of the respondents. This data was collated using the 'About You' questions in the second part of the questionnaire. These questions were optional. A total of 13 respondents answered these questions.

4.3.1 Age

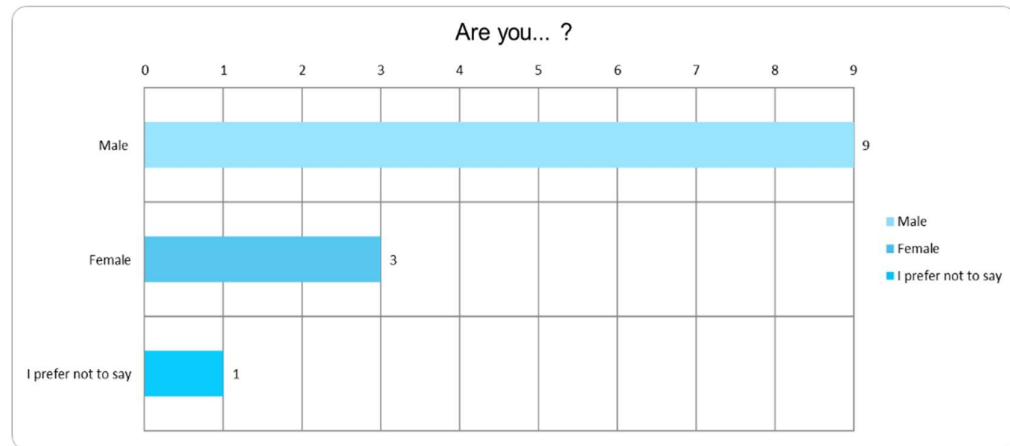
- 1 respondent were aged between 25-34
- 2 were aged between 35-49
- 6 were aged between 50-59
- 3 were aged between 65-74
- 1 respondent was aged between 75-84.



Church Road and Tonge Road, Sittingbourne Traffic Calming Scheme

4.3.2 Sex

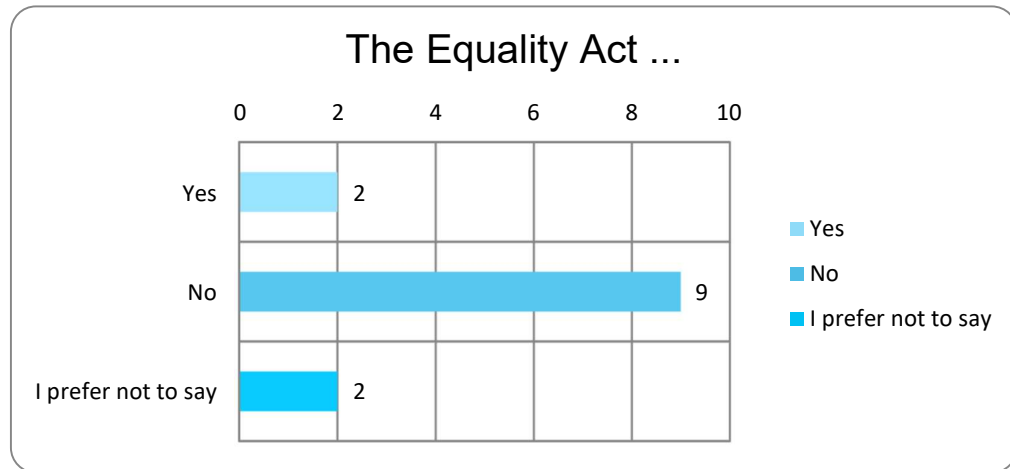
- 9 respondents were male
- 3 respondents were female
- 1 preferred not to say.



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4.3.3 Disability

- 9 respondents did not consider themselves having a disability
- 2 respondents preferred not to say and
- 2 respondents said yes.



5 Next Steps

Following the feedback from this consultation, the design team will be carefully considering points raised to establish whether further design changes are appropriate. KCC recommendation is to progress the scheme to detailed design and construction. The raised table proposal will reduce traffic speeds on Church Road and Tonge Road. The measures will also provide improved crossing facilities for pedestrians and reduced crossing distances at Portland Avenue and Oak Road. The current parking issues may be improved by adding more parking options at the electrical distribution plant location however this is not being promoted or progressed by KCC at this time.

This consultation report will be presented, along with the detailed design, at the next Swale Joint Transportation Board (JTB) which is due to be held on the 20 June 2022 and a recommendation will be taken regarding how to proceed with the scheme.

If the decision is taken to proceed with the scheme construction is likely to start late summer 2022 and a Start of Works Notice will be published to inform residents and members of the public.

This report is available on our website www.kent.gov.uk/tongeroadtrafficalming and we will send a notification to those who have provided contact details throughout the process, including stakeholder organisations.

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M2 junction 5 improvements scheme update – May 2022

Construction update

Our compound is now up and operational, which can be spotted from the A249, north of Stockbury roundabout. We have clearance and preparation works happening on most approaches to the roundabout, making the most of the mil winter and spring we had to carry out as much earth works as possible.

We have had a closure on Oad Street in place since April 25. During this time, we have widened the carriageway, installed new drainage and kerbing, and surfaced part of the road. (Pictured right, Oad St just before surfacing)



Progress on the Maidstone Road/Oad Street link taken from the top of the hill looking down towards the A249.



Progress on the Maidstone Road/Oad Street link taken from our site compound looking up.

What’s happening over the next couple of months

We have some weekend lane and total closures on the A249 southbound carriageway, north of Stockbury roundabout in June. More information on these is detailed on our [project website](#).

We are making progress on the Maidstone/Oad Street link road. We will be installing drainage on the A249 southbound carriageway, north of the junction, and we will be lifting our closure on Oad Street on 20 June. The closure will return in the autumn and when we have finalised dates for these they will be shared on our website.

Our piling work continues on the western verge of the A249 northbound, preparing for a retaining wall as part of the link roads. Over the next few weeks, you will see preparation works start to happen on the roundabout as well as on a second location under the bridge.

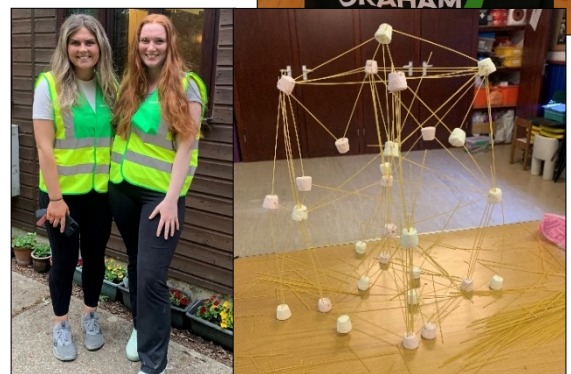
In the community

Every day's a school day for our social value and stakeholder team. Sandra Saxton and Ellen Dennison visited Westlands School's careers fair in April to talk to students about careers in construction and engineering.

From year 7 to year 13, it was great to see so many young people engaged and interested in a career in infrastructure.

Brownie's promise! With the helping hand of Site Engineers Georgina Watson and Ciara McAuley, the Maidstone Brownies showed us how to build self-supporting structures with just marshmallows and spaghetti.

In small groups of six, they worked together to try and build the tallest tower. We were very pleased to help the girls on their way to their "Think Like an Engineer" badge.



Thank you to Stockbury Parish Council. In April, we joined them in the community bus for a guided tour of the lane network in and around Stockbury.

It was very helpful for us to gain a better understanding of the routes and the safety concerns locals have on these routes.

Meet the team!

Want to know more about the scheme and upcoming closures?

Our Stakeholder and Communications Officer will be at the Oad Street Food and Crafts between **10am and 12pm and then again from 5pm and 7pm, on Wednesday 13 July** to meet with you. There is parking on site and the café can be found on Oad Street, Sittingbourne ME9 8LB

We are on hand to answer any questions you have about the timeline of the project, the closures we have planned, or local community engagement opportunities. This will be the first of our monthly drop in clinics, which will be held on the morning of every second Wednesday of the month, sign up to our [project website](#) for updates of where it will be held each month.

Facebook - @HighwaysSEas
Twitter - @HighwaysSEAST

(Please don't add page numbers or an item number and delete this note)

A249 Key Street and Grovehurst Road junction Improvements

To: **Swale Joint Transportation Board – 20 June 2022**

Main Portfolio Area: **Growth, Environment & Transport**

By: **Tim Read, Head of Transportation**

Classification: **For Information**

Ward:

Division: **Swale West and Sittingbourne North**

Summary: Update on the proposed junction improvements on the A249 at Key Street and Grovehurst Road

1.0 Introduction

- 1.1 One of the outcomes of the Swale Local plan adopted 2017 was the need to improve the junctions along the A249 at M2J5, Key Street and Grovehurst Road. The improvements would support the delivery of over 6,000 homes in the period 2022 – 2031 identified in the adopted Swale Local Plan, reduce existing congestion and queuing, as well as improving facilities for pedestrians and cyclists.
- 1.2 A detailed funding bid was submitted and in November 2019, Homes England, the non-departmental public body under MHCLG announced that the bid had been successful and £38.1m of grant funding had been allocated for both junction improvements. A Grant Determination Agreement (GDA) between KCC and Homes England was signed in September 2020. A condition of the GDA is that funding was due to be spent by 31 March 2024, however an extension has been requested following the delays in confirming the M2J5 scheme which was also a condition of release of the HIF funding.
- 1.3 This report gives an update on the A249 Junction improvement Schemes at.
 - Key Street
 - Grovehurst Road
- 1.4 Approval to progress these proposals was confirmed following the Environment & Transport Cabinet Committee meeting in March 2020
- 1.5 The M2J5 scheme is being delivered by National Highways (formally Highways England). Following a public inquiry, the Secretary of State published a decision approving the project in June 2021.

2.0 Current Situation

- 1.0 Systra have been employed as the consultants to undertake the transportation assessments and develop the detailed designs for these schemes.

Key Street

- 1.1 The A249 Key Street junction is a two-bridge oval shaped roundabout junction with slip roads on the southern side and connections to the A249, A2 Keycol Hill, Bobbing Hill, Sheppey Way and Chestnut Street. It serves the A2, village communities, and Sittingbourne town centre.
- 1.2 The scheme for Key Street is to improve capacity and safety on the existing Key Street gyratory and to relocate the access to the south bound slip road to the A249 along Chestnut Street, giving due consideration to the proposed access to Wises Lane development granted planning consent by the Secretary of State following an appeal. A new footway cycleway and controlled crossing provision is included along Chestnut Street. The proposals are shown on the scheme plan in Appendix A.
- 1.3 The new roundabout on Chestnut Street is due to be provided as part of the Wises Lane development. Details of the proposals are required to be submitted to Swale Borough Council Planning to approve and discharge the reserved matters – 22/500782/SUB
- 1.4 The connection onto the A249 from the new roundabout will be delivered as part of the Key Street Improvement works.
- 1.5 These improvements are considered permitted development, so a planning consent is not required.

Grovehurst Road

- 1.6 The A249 Grovehurst Road junction is a double roundabout layout with a single two lane carriageway bridge over the A249 connecting the two. It is the main route for traffic heading towards the Sittingbourne Northern Relief Road to the east and Iwade village to the west. The junction also has connections to Grovehurst Road and Swale Way
- 1.7 The scheme for Grovehurst Road is to provide an additional bridge over the A249 creating a full gyratory junction. The pedestrian and cycle facilities included signal controlled crossings within the junction have been revised following comments received during the recent public consultation exercise. The proposals are shown on the scheme plan in Appendix B
- 1.8 The land required for the gyratory has been safeguarded through voluntary agreement and s106 agreements. Heads of Terms have been agreed and the acquisition will be progressing via solicitors.
- 1.9 Planning consent for the scheme was granted 16 March 2022.

- 1.10 Further extension of the Homes England funding has been agreed and is now available until 31 October 2024.
- 1.11 Advance works on the site are underway, with the Ground Investigation completed in March 2022. Archaeological investigation was completed April 2022, although further mitigation works will be required during construction. Water Voles have recently been identified on site and the required mitigation strategy has been developed. The strategy requires a Natural England Licence, with translocation planned for February/March 2023. A Great Crested Newt (GCN) licence has been obtained from Natural England and a reptile mitigation translocation site identified at Iwade Nature Park with translocation planned for Autumn 2022. Environmental enhancement works will be undertaken during June 2022, this includes the creation of hibernacula, scrub clearance and grassland enhancement.
- 1.12 It is noted that there has been an overlap between the planning application for the KCC Highway Improvements scheme ref SW/21/505738 and the Land east of Iwade development planning ref 19/503974/HYBRID which has a resolution to grant consent subject to signing of the section 106 agreement. To reduce the impact of traffic management on Grovehurst Road it is proposed to include the development access roundabout as part of this scheme, with the developer paying for the cost of this work through a s278 agreement.

A249 and National Highways and Interface with the M2J5 Project

- 1.13 The A249 from the M2 to Sheerness is managed under a Design, Build, Finance and Operate (DBFO) Contract with National Highways. Any amendments, improvements or additions to the A249 that influence the route will be subject to agreement by National Highways and the DBFO contractor.
- 1.14 Discussions have been held with representatives of National Highways and the contractor for the M2J5 improvements to consider programme and traffic management implications for the delivery of these projects within the funding timeframe.

3.0 Programme

- 3.1 An interim scheme to install traffic signals at the junction of the A249 north bound exit slip road and the Key Street Gyratory was completed 07 August 2020.
- 3.2 The current programme is;
Key Street

Complete detailed design	October 2022
Procurement (phase 1)	June 2022 to August 2022
Procurement (phase 2)	September 2022 to December 2022
Construction of Chestnut St roundabout (by others)	December 2022 to June 2023
Contract award	March 2023
Commence works	July 2023
Works completed	August 2024

Complete detailed design	October 2022
Confirm s106 contributions and land agreements	June 2022
Procurement (phase 1)	June 2022 to August 2022
Procurement (phase 2)	September 2022 to December 2022
Contract award	March 2023
Commence works	April/May 2023
Works completed	August 2024

3.3 Consideration is being given to reduce the impact that construction works will have of the A249 and the local highway network. The timing and traffic management for both the improvements at Key Street and Grovehurst Road works will be planned in detail with the KCC Streetworks Co-ordinator, National Highways and adjacent developments

4.0 Communications

4.1 A public consultation was undertaken. The consultation ran for eight weeks from Wednesday 2 December 2020 until Tuesday 26 January 2021. Details of the consultation and the subsequent reports are available to view www.kent.gov.uk/a249swalejunctionimprovements.

4.2 A Communications Plan is being developed. This will involve informing residents, road users and key organisations of the proposals and timescale for each of the phases of the project.

4.3 Notification of the roadworks will be provided prior to commencement of construction using temporary variable message signs, and by a letter drop to local residents.

4.4 A steering group has been established, to ensure stakeholders are kept updated on the progress of the project.

5.0 Financial

Key Street

5.1 The cost estimate for the improvements to Key Street is £5.9m.

5.2 Contributions of £0.5m have been collected from the developments at

- High St Newington planning reference 16/501266
- Spirit of Sittingbourne, planning reference 14/505440,
- Crown Quay, planning reference 16/507877

5.3 The balance of £5.4m being funded by the Housing Infrastructure Fund

5.4 The new roundabout on Chestnut Street is due to be provided as part of the Wises Lane development.

Grovehurst Road

- 5.5 The cost estimate for the improvements to Grovehurst Road is £32.7m, which is being fully funded by the Housing Infrastructure Fund
- 5.6 The current cost estimate is robust and is based on outline proposals that have been reviewed by external cost consultants. It includes:
- construction costs
 - budget estimates from the utility companies
 - project management costs
 - allowances for contingencies, inflation, and risk
- 5.7 It is anticipated that further s106 developer contributions will be available to cover any shortfall that may materialise. However, the development of the outline designs and later detailed design and construction procurement strategy will all be aimed at achieving value engineering savings and optimising costs.

Recovery Strategy

- 5.8 As part of the HIF funding agreement there is a recovery strategy that will claw back developer contributions from developments that have a traffic impact on the Key Street and Grovehurst Road junctions and that would have been expected to contribute to the improvements. There is an expectation that this could raise in the region of £8.0m that will be used for the purposes of investing into much needed infrastructure within the Borough of Swale.

6.0 Legal implications

- 6.1 This Report is for information only and hence there are no legal implications for the Board.

7.0 Conclusions

- 7.1 The successful award of the Homes England HIF funding, the commitments to make all the land available and match funding from s106 agreements will allow the highway improvements of the A249 Junctions at Key Street and Grovehurst Road to be delivered, in support of the Swale Local Plan; Bearing Fruits 2017.

8.0 Recommendations

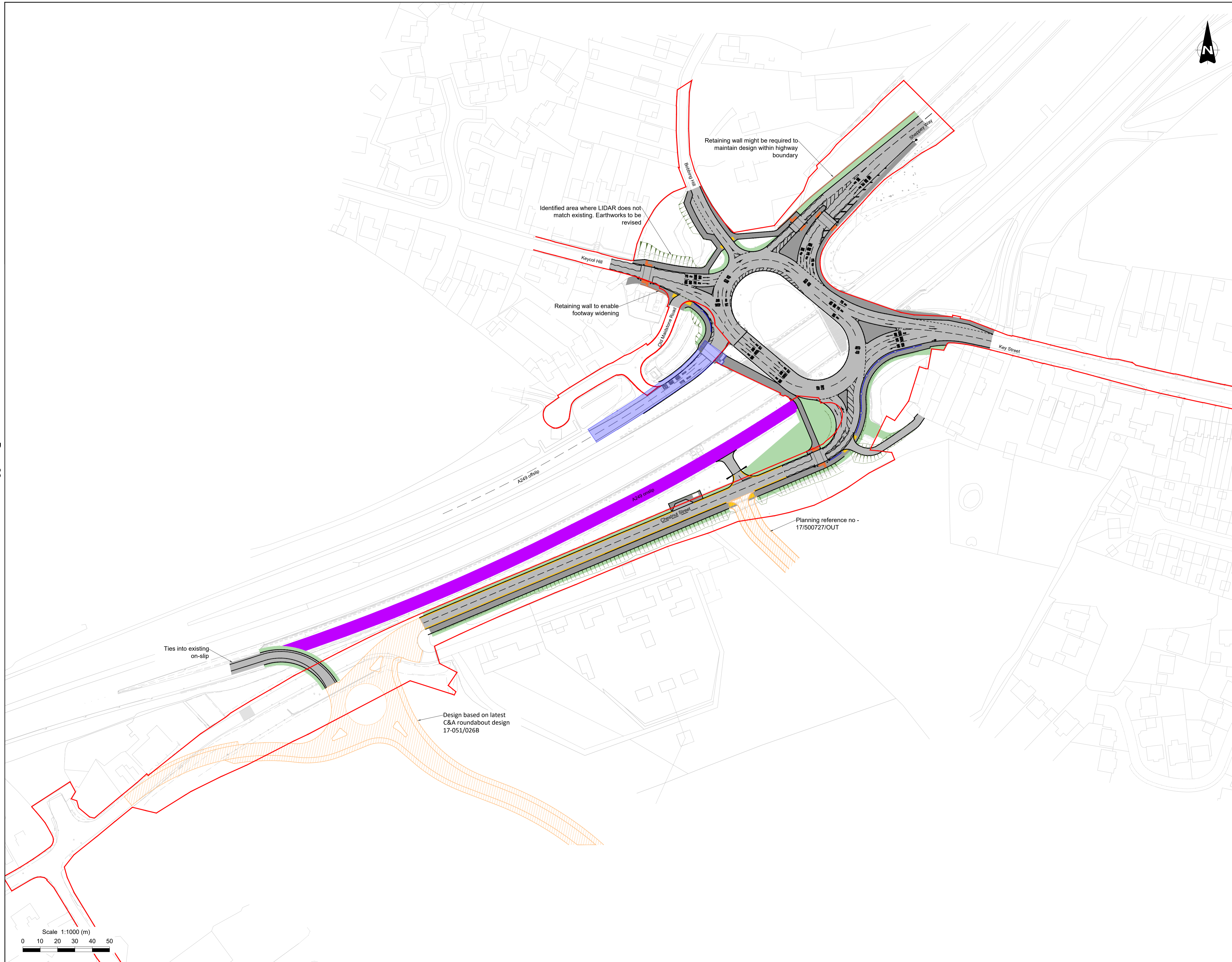
For Information

Future Meeting if applicable: As necessary after planning and prior to construction.	Date: TBA
--	-----------

Contact Officer:	Richard Shelton - Project Manager (Major Capital Programme Team) e mail: Richard.Shelton@kent.gov.uk tel: 03000 419550
Reporting to:	Tim Read – Head of Transportation

Appendices

Appendix A	Key Street General Layout Plan - with Wises Lane development roundabout on Chestnut Street - Drawing no. 108945-dwg-ks-101-01 to 03 rev P2 (DRAFT)
Appendix B	Grovehurst Road Scheme Plan – Drawing no. 109617-dwg-101-01 rev P6



- Notes:
1. Do not scale from this drawing. If in doubt refer to the project manager for clarification.
 2. All dimensions are shown in metres unless otherwise stated.
 3. This drawing forms part of a design pack and should be read in conjunction with all drawings listed on the project drawing register.
 4. All works shall be carried out in accordance with the approved drawings and specifications to the satisfaction of the Highway Authority.
 5. All Temporary Traffic Management shall fully comply with the Traffic Signs Manual, Chapter 8.
 6. All signs, bollards and other street furniture shall maintain a minimum clearance from kerb face of 450mm.
 7. Base mapping and topographical survey based on J. C. White Geomatics Limited Drawing 2000188 - Key Street - Roundabout_2D_10_REV_B as supplied by Kent CC.
 8. For signal layout/design and setting out refer to KCC Drawing KCC/ITS/2019/0161/S/1.
 9. 3D design and earthworks extensions based on LIDAR information which is subject to alteration based on existing vegetation.

- Key:
- OS
 - Design
 - Proposed road marking
 - Highway boundary
 - Proposed guardrail
 - Proposed verge
 - Existing slip road to be closed. Treatment to be confirmed
 - Designed and built by others
 - Proposed footway
 - Proposed carriageway
 - Proposed tactile paving for uncontrolled crossing
 - Proposed tactile paving for toucan crossing
 - Proposed earthworks
 - Work constructed as part of Key Street advanced works scheme
 - Proposed retaining wall

P2	20/05/22	Updates following new topo	RM	ARM	JG	JG
P1	11/01/20	Design updated following consultation	ARM	EH	EH	JG
P0	24/08/20	Initial issue	ARM	EH	DC	DM
Rev	Date	Revision details	Drawn	Check	Review	Approv

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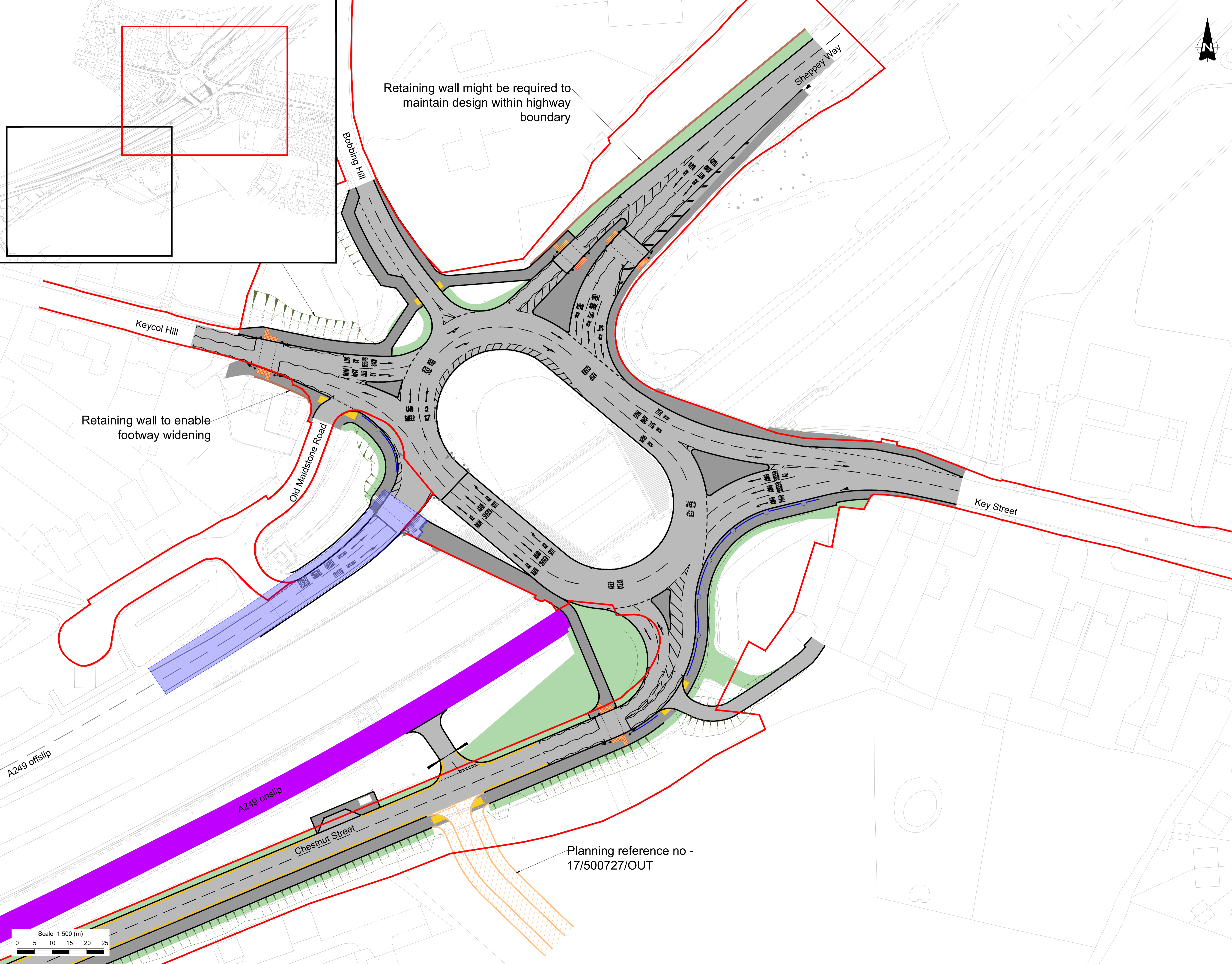
Client

 Kent County Council
 kent.gov.uk

Project
Key Street Roundabout Improvements

Title
General Arrangement (Sheet 1 of 3)

Drawn	ARM	Checked	EH	Reviewed	DC	Approved	DM	
Original Status	AT	Date	24/08/2020	Scale	1:1000	Drawing Status	Preliminary	
Drawing Number	108945-dwg-ks-101-01						Rev.	P2



Retaining wall might be required to maintain design within highway boundary

Retaining wall to enable footway widening

Planning reference no - 17/500727/OUT

- Notes:
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 9. 3D design and earthworks extensions based on LIDAR information which is subject to alteration based on existing vegetation.

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 - Highway boundary
 - Proposed guardrail
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 - Proposed earthworks
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P2	20/08/22	Updates following new topo	RM	ARM	JG	JG
P1	11/01/20	Design updated following consultation	ARM	EH	EH	JG
P0	24/08/20	Initial issue	ARM	EH	DC	DM
Rev	Date	Revision details	Drawn	Check	Review	Approv

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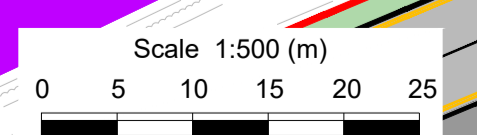
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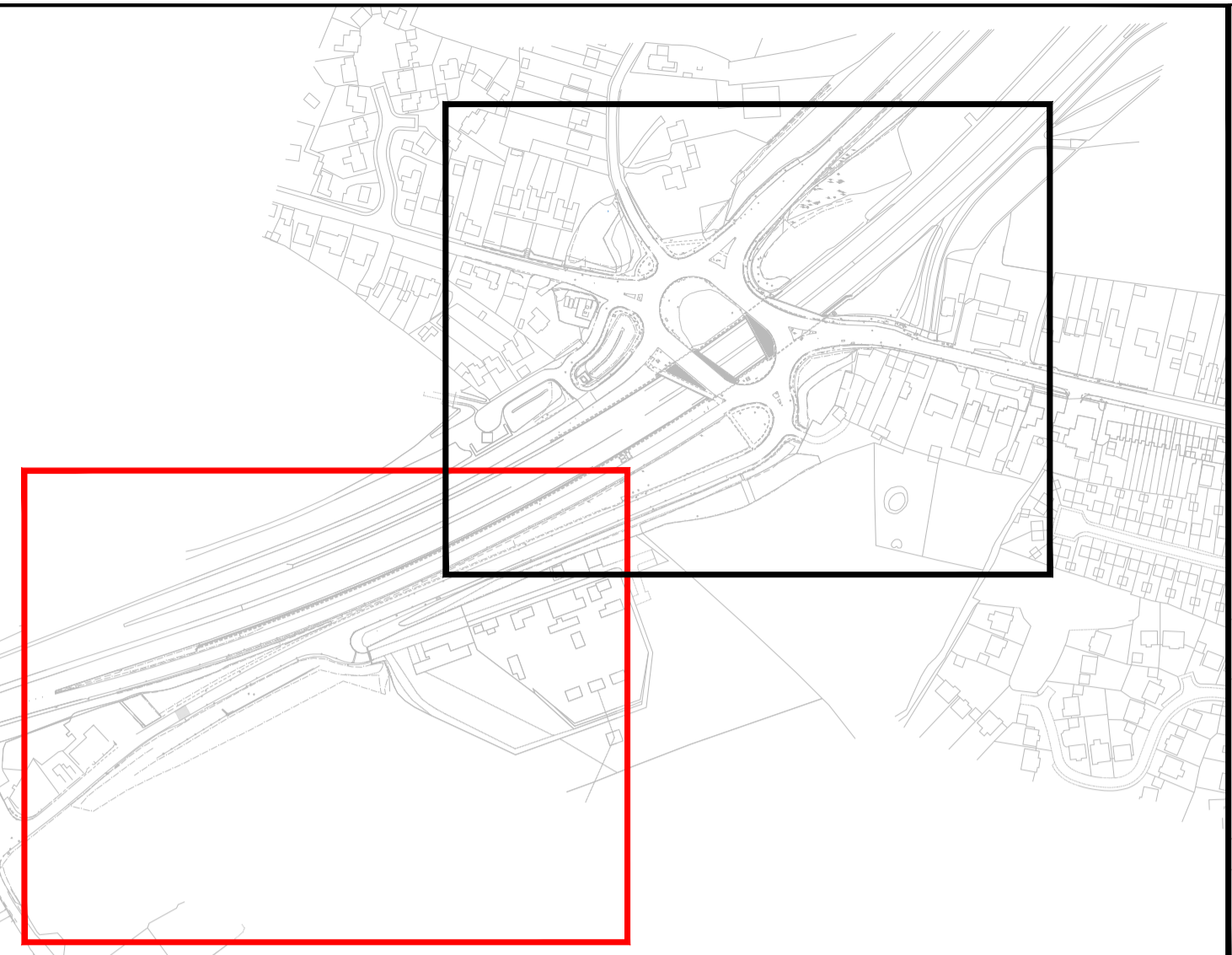
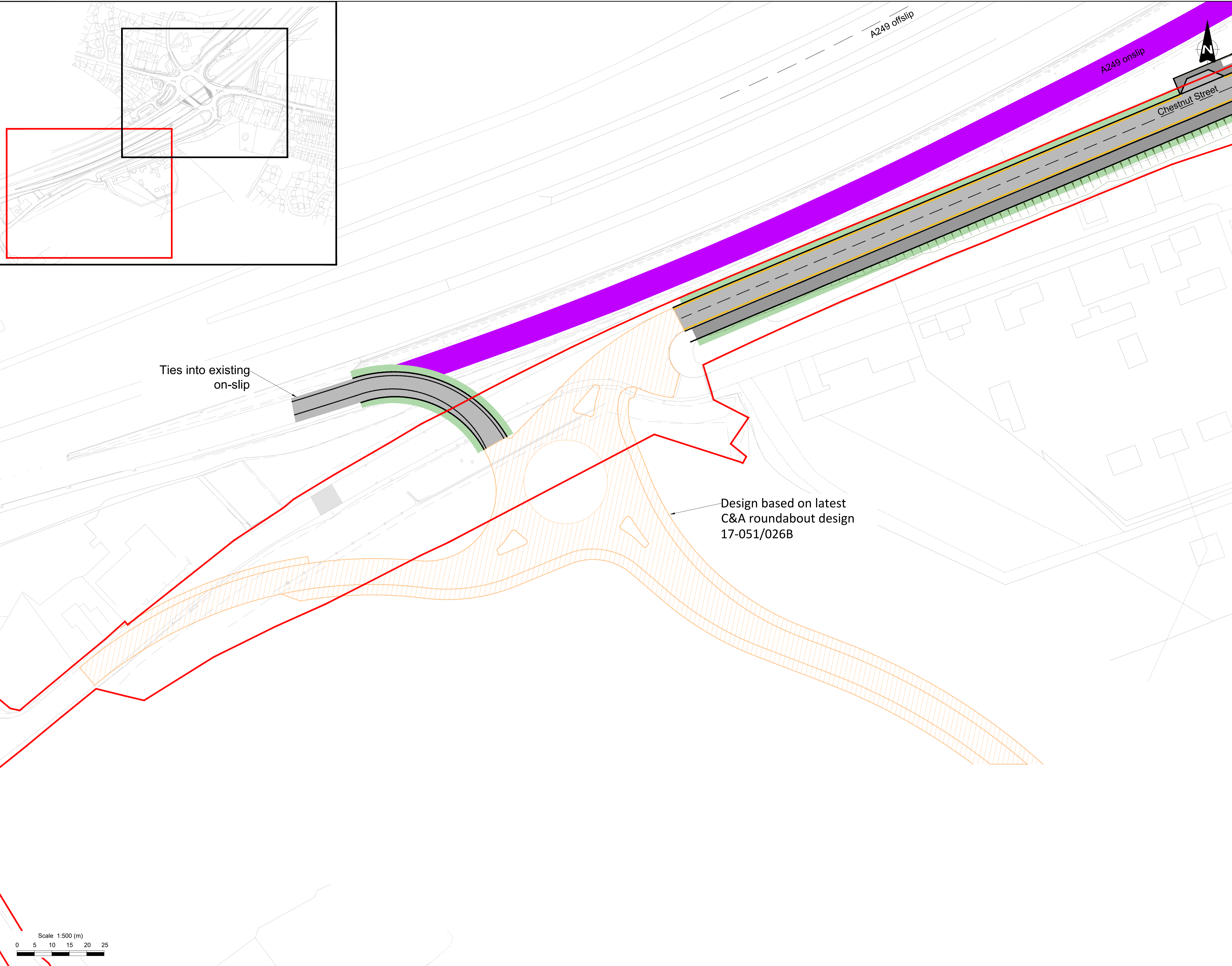


Project
Key Street Roundabout Improvements

Title
General Arrangement (Sheet 2 of 3)

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Original Drawing	AT	Date	24/08/2020	Scale	1:500	Drawing Status	Preliminary	
Drawing Number	108945-dwg-ks-101-02						Rev.	P2





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 3. This drawing forms part of a design pack and should be read in conjunction with all drawings listed on the project drawing register.
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 8. For signal layout/design and setting out refer to KCC Drawing KCC/ITS/2019/0161/S/1.
 9. 3D design and earthworks extensions based on LIDAR information which is subject to alteration based on existing vegetation.

- Key:
- OS
 - Design
 - Proposed road marking
 - Highway boundary
 - Proposed guardrail
 - Proposed verge
 - Existing slip road to be closed. Treatment to be confirmed
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P2	20/08/22	Updates following new topo	RM	ARM	JG	JG
P1	11/01/22	Design updated following consultation	ARM	EH	EH	JG
P0	24/08/20	Initial issue	ARM	EH	DC	DM
Rev	Date	Revision details	Drawn	Check	Review	Approv

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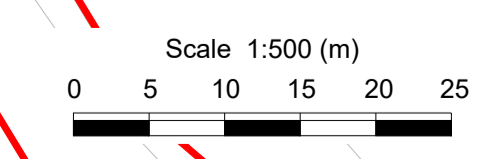
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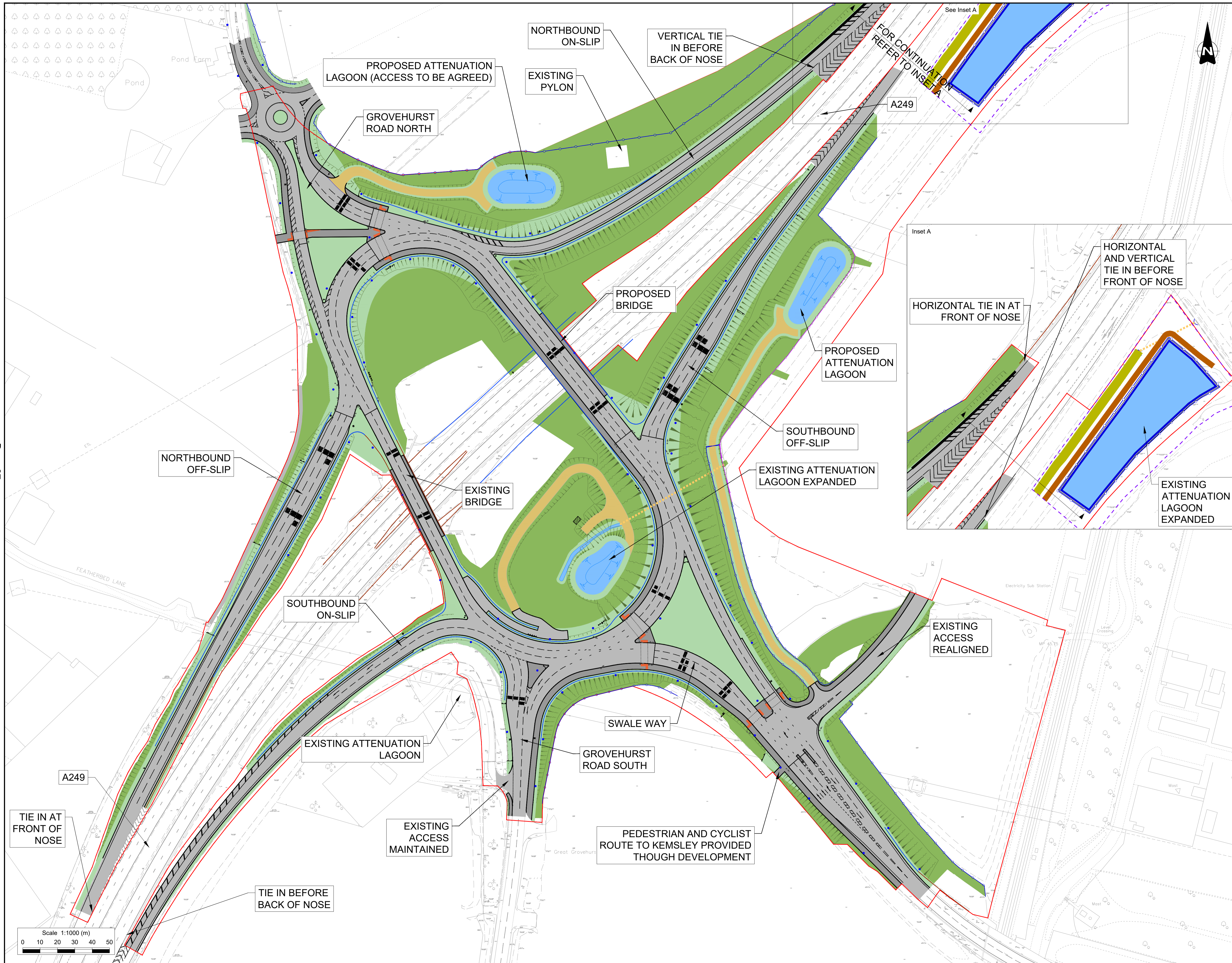
Project
Key Street Roundabout Improvements

Title
**General Arrangement
(Sheet 3 of 3)**

Drawn	ARM	Checked	EH	Reviewed	DC	Approved	DM	
Original Scheme	AT	Date	24/08/2020	Scale	1:500	Drawing Status	Preliminary	
Drawing Number	108945-dwg-ks-101-03						Rev.	P2

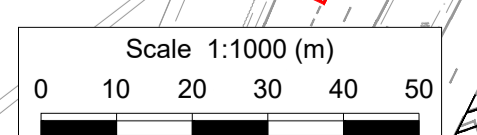
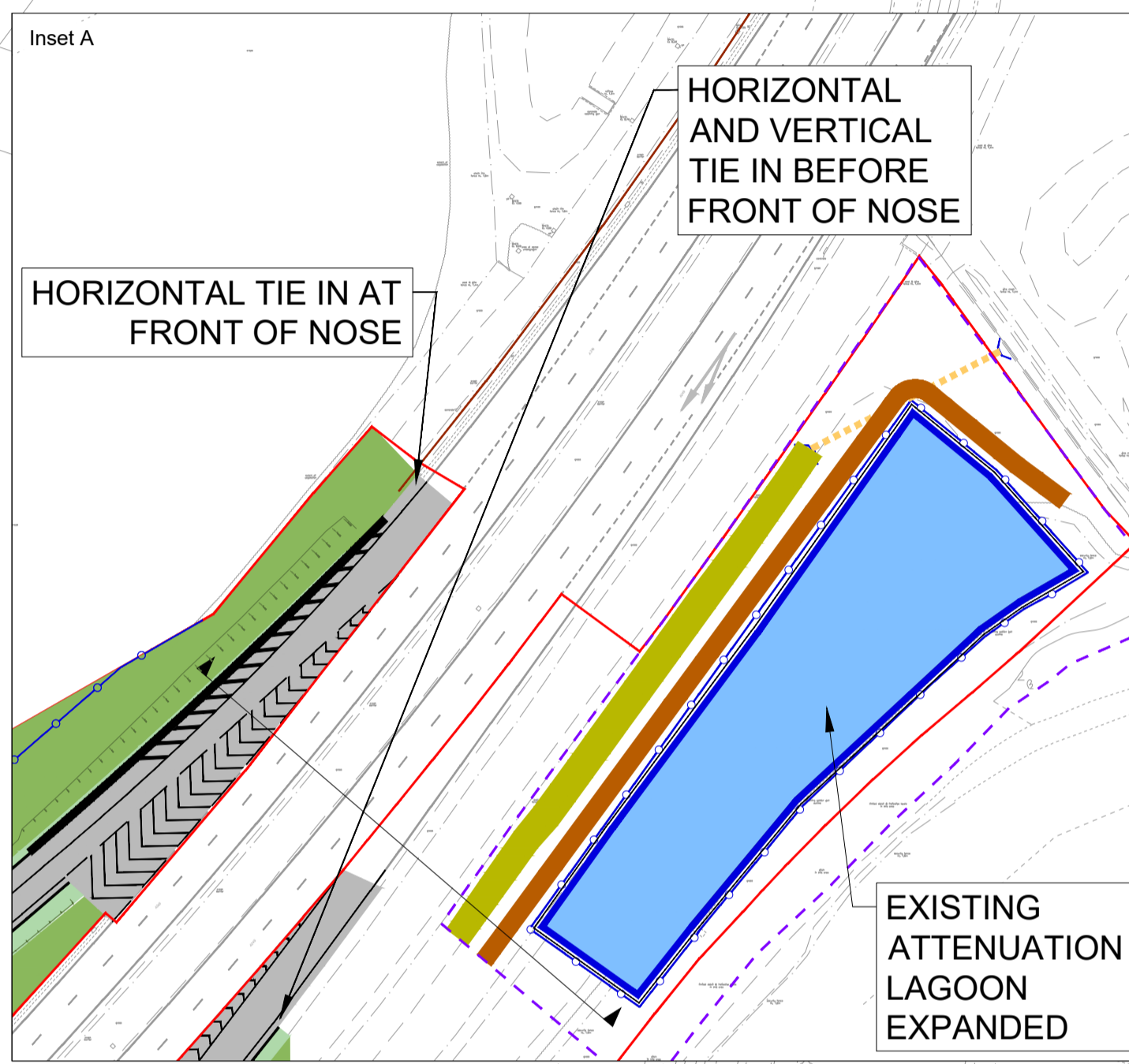


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 2. All dimensions are shown in metres unless otherwise stated.
 3. Layout based on Topographical survey from JC White on 22-03-2021. And based on Ordnance Survey MasterMap, © Crown Copyright 2018. All rights reserved. Licence number 100022432.
 4. This drawing forms part of a design pack and should be read in conjunction with all drawings listed on the project drawing register.

- Key:
- OS mapping / topographical survey
 - Site Boundary
 - Proposed kerb
 - Proposed road marking
 - Proposed carriageway
 - Proposed shared footway
 - Proposed verge / area kept clear for visibility
 - Proposed green space / landscaping
 - Proposed earthworks / embankments
 - Proposed grasscrete
 - Proposed attenuation pond
 - Proposed tactile paving
 - Proposed sign
 - Proposed VRS / bridge parapet
 - VRS / bridge parapet to remain
 - Proposed wooden post fence / highway boundary
 - Proposed steel security fence / highway boundary
 - Proposed drainage culvert
 - Consented watercourse/diversion route
 - Proposed re-alignment to permissive footway route
 - Proposed gabion retaining wall
 - Proposed signal head
 - Proposed lighting column
 - SGN building
 - Nicholls Pond Planning Area



Rev	Date	Revision details	Drawn	Check	Review	Approv
P6	24/05/2022	Nicholls Attenuation Lagoon amended to Aecom drawing 60472559_ACM_XX_XX_DR CE_1004	MIE	EH	NL	JG
P5	24/09/2021	Updated to reflect latest alignment design	RM	DH	DC	DC
P4	02/09/2021	Updated to include Nicholls updates and other design changes	DH	EH	DC	DC
P3	15/07/2021	Updated to reflect latest alignment design	RM	DH	DC	DC
P2	17/05/2021	Updated to reflect latest pond arrangements	DH	EH	DC	DC
P1	05/08/2020	Updated to reflect latest alignment design	DH	EH	DC	DC
P0	03/06/2020	Initial Issue	DH	EH	DC	DC

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Project: **Grovehurst Outline Design**

Title: **Proposed Layout General Arrangement**

Drawn	Checked	Reviewed	Approved
DH	EH	DC	DC

Original size: A1 Date: 03-06-2020 Scale: 1:1000 Drawing Status: Preliminary
 Drawing Number: 109617-dwg-101-01

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Part 6 Amendments

JTB Briefing

Background

The Traffic Management Act (TMA) 2004 introduced civil enforcement of traffic offences in England and Wales.

Part 6 of the Act allows councils outside of London to enforce moving traffic offences, making banned turns, exceeding weight limits, and stopping in yellow box junctions.

Despite the 2004 Act now being 18 years old, secondary legislation has never been introduced to parliament and moving traffic offences can only be enforced by the police under criminal law. New legislation has now been laid before parliament and is due to come into force from 1st June 2022.

The Government announced their intention to enact Part 6 of the 2004 Traffic Management Act in the “Gear Change: A bold vision for walking and cycling” strategy. The Department for Transport (DfT) is allowing applications to be submitted by Local Highway Authorities for the first tranche until 20th May 2022 and following approval will lay the 1st designation orders in parliament during the Summer of 2022. This is all subject to the Parliamentary timetable. Kent County Council (KCC) are on track to apply for these powers during the first tranche. Applications for latter tranches are expected to be invited later in the year.

The DfT have confirmed that the Minister has approved a prescribed list of Traffic Signs (restrictions) and that the powers, when designated, will permit civil enforcement using certified camera devices. These powers do not replace those already held by the Police, who would also be able to carry out enforcement.

DfT

The DfT announced in September 2020 that they would be fully enacting the remaining elements of Part 6 of the TMA, permitting local authorities outside of London to use approved camera devices to enforce moving traffic contraventions.

By introducing the enforcement of moving traffic contraventions and utilising the latest ANPR camera technology, authorities will be in a far better position to manage and improve their local road network and deliver key objectives outlined in their local transport plans.

KCC Proposal

Implementation and operation of moving traffic enforcement will be managed on a countywide approach undertaken by KCC which will strengthen the Local Highway Authority’s statutory duty in meeting the requirements placed upon it under the Traffic Management Act 2004: to reduce traffic congestion in towns and cities and manage the road network.

This will deliver the DfT’s requirement for Local Highway Authorities to provide a consistent, controlled enforcement of the highway network, that is in line with the general principles of good regulation. Enforcement under these powers will be carried out in a way which is transparent, accountable, proportionate, and consistent; and will only target locations where action is needed.

As part of the KCC application, a public consultation exercise took place between 15th March and 9th May 2022 in line with DfT guidance. KCC are now categorising and reviewing all feedback either positive, negative or neutral. Initial indications show support for the Local Highway Authority to take on the enforcement powers. KCC will then consider any objections and identify if there are any

reasonable actions that can be taken to resolve those objections. Following the review of the feedback, KCC will present the findings to KCC's Environment & Transport Committee, with the finalised report to be published on the KCC website.

Kent Network Benefits of moving traffic enforcement

KCC will use the enforcement of moving traffic contraventions to:

1. Improve road safety
2. Reduce network congestion
3. Increase public transport reliability
4. Improve Air Quality
5. Increase the lifespan of highway assets

Moving traffic enforcement will only be applied where action is needed to meet one of these objectives.

Key Dates

Activity	Timeframe
Public engagement on 1 st tranche sites	15 March to 9 May 2022
KCC to submit application to DfT	May 2022
Feedback provided to the public engagement	Summer 2022
Legislation passes through Parliament	Summer / Autumn 2022
KCC gains powers to begin enforcement	Late 2022

Feedback

For further information on KCC's proposal, the prescribed list of Traffic Signs (restrictions) allowed under civil enforcement, and the first tranche of sites, please see KCC's public engagement website:

www.kent.gov.uk/trafficmanagementact

To: Swale Joint Transportation Board
By: KCC Highways, Transportation & Waste
Date: 20th June 2022
Subject: Highways Forward Works Programme: 2022/23
Classification: Information Only

Summary: This report updates Members on the identified schemes approved for construction

1. Introduction

This report provides an update and summarises schemes that have been programmed for delivery in 2021/22 and 2022/23.

Kent County Council has recently published a forward works programme for the next five years covering planned maintenance of our highway assets. It is in two parts: the first concerns the next two years (2021/22 - 2022/23), and most of the sites included have already been verified by our engineers. The second part relates to years three to five of our five-year programme (2023/24 - 2025/26), and is largely based on data from our asset management systems, so may be subject to more changes as the schemes are verified.

This programme is subject to regular review and may change for a number of reasons including budget allocation, contract rate changes, and to reflect our changing priorities. The programme and extent of individual sites within the programme may also be revised following engineering assessment during the design phase, and additional sites may be added or others advanced if their condition deteriorates rapidly so that we need to react in order to keep the highway in a safe and serviceable condition.

Further information about how we manage our highway infrastructure, including our county-wide five-year forward works programme, may be found on our website:

<https://www.kent.gov.uk/about-the-council/strategies-and-policies/transport-and-highways-policies/managing-highway-infrastructure>

In addition to planned maintenance of our highway assets, this report includes transportation and safety schemes, developer funded works, Combined Members Grant schemes, and planned maintenance of public rights of way.

Road, Footway & Cycleway Renewal and Preservation Schemes – see Appendix A

Drainage Repairs & Improvements – see Appendix B

Street Lighting – see Appendix C

Transportation and Safety Schemes – see Appendix D

- **Casualty Reduction Measures**
- **Externally funded schemes**

Developer Funded Works – see Appendix E

Bridge Works – see Appendix F

Traffic Systems – see Appendix G

Combined Members Grant – Member Highway Fund – see Appendix H

Public rights of way – see Appendix I

Conclusion

1. This report is for Members' information.

Contact Officers:

The following contact officers can be contacted on **03000 418181**

Richard Emmett	Highway Manager Mid Kent
Alan Blackburn	Swale District Manager
Alan Casson	Strategic Asset Manager
Earl Bourner	Drainage Asset Manager
Neill Coppin	Structures Operations Team Leader
Sue Kinsella	Street Light Asset Manager
Toby Butler	Traffic & Network Solutions Asset Manager
Jamie Hare	Development Agreements Manager
Jamie Watson	Schemes Programme Manager

Appendix A – Footway and Carriageway Improvement Schemes

The delivery of these schemes is weather dependent; should it prove not possible to carry out these works on the planned dates, new dates will be arranged and the residents will be informed by a letter drop to their homes.

Machine Resurfacing – Contact Officer Byron Lovell			
Road Name	Parish	Extent of Works	Current Status
Canterbury Road (Boughton Hill)	Dunkirk	<u>Stabilisation Works</u> East of Staplestreet Road	Design work being carried out. Works maybe delayed due to high material costs
B2008 Minster Road	Minster-On-Sea	Between Halfway Crossroads to Barton Hill Drive	Programmed 8 th June 2022
High Street / West Street	Sittingbourne	From Park Road to Central Avenue	Programmed 13 th July 2022
A251 Stone Street	Faversham	Between South Road and Preston Street	Programmed 1 st June 2022
A249 Key Street Roundabout	Sittingbourne	Circulatory of Roundabout	Programmed 23 rd August 2022
A2 Keycol Hill	Bobbing	100m approach and exit with roundabout with A249 Key Street roundabout	Programmed 19 th August 2022
A2 London Road	Tonge	Dully Road to Panteny Lane	Programmed 15 th August 2022
Borden Lane	Sittingbourne	Homewood Avenue to Riddles Road	To be programmed
Footway Improvement - Contact Officer Neil Tree			
Road Name	Parish	Extent and Description of Works	Current Status
Queensway and Coats Avenue (Phase 2)	Sheerness	<u>Footway Reconstruction</u> Entire extent of Coats Avenue and the remaining sections of Queensway not completed in Phase 1	Completed.

Frensham Close	Sittingbourne	<u>Footway Reconstruction</u> Sections within Frensham Close and the "inner footway" within the court.	To be designed and programmed
London Road	Sittingbourne	<u>Footway Reconstruction</u> Sections to be identified at design stage in the vicinity of Chalkwell Road and Borden Lane.	To be designed and programmed
Minster Road	Minster-on-Sea	<u>Footway Protection Treatment</u> Barton Hill Drive to Scrapsgate.	Designed and to be programmed
Canterbury Road	Faversham	<u>Footway Protection Treatment</u> From the junction of Love Lane to Preston Avenue	Designed and to be programmed

Surface Treatments - Contact Officer Jonathan Dean

Road Name	Parish	Extent of Works	Current Status
CHALKWELL ROAD	Sittingbourne	From A2 London Road to B2006 Roundabout	Programmed 15 th August 2022
THE STREET	Lower Halstow	Vicarage Road to 30mph Gateway	Programmed 11 th June 2022
B2231 SHEPPEY WAY	Minister	A2500 Lower Road to Old Ferry Road (Not including Bridge)	Programmed 6 th June 2022
B2008 CHEQUERS ROAD	Minster-On-Sea	Back Lane to Plough Road	Programmed 11 th August 2022
BORDEN LANE (INC THE STREET)	Borden	A2 to Pond Farm Road	Programmed 8 th August 2022

STAPLE STREET	Boughton Under Blean	Staple Street Road to Church Hill	Programmed in July 2022
TUNSTALL ROAD	Tunstall	From Cranbrook Drive to Hearts Delight Road	Programmed in July 2022

Appendix B – Drainage Repairs & Improvements

Drainage Repairs & Improvements - Contact Officer Earl Bourner			
Road Name	Parish	Description of Works	Current Status
A2 Canterbury Road	Snipeshill, Sittingbourne	Flood and Water Management Team and Highways Joint assessment of existing drainage system at open space by Greenways.	KCC FWM Team progressing design and applications for external funding.
Blind Mary's Lane / Swanton Street	Bredgar	Improvements to existing gully system following previous soakaway improvement	Works Completed
Lansdown Road & Coombe Drive	Sittingbourne	Phase 2 works near Woodberry Drive for additional new drainage to reduce flood risk at Lansdown Road, in turn alleviating flood risk from overland flow passing down into Coombe Drive.	Preparing for contractor handover. Delivery Programme to be confirmed
Bull Lane	Newington	Desilting of existing drainage pond	Works deferred until further notice
Tonge Corner Road	Tonge	Additional drainage improvement to reduce surface water flood risk to property	Investigation works completed. No further works planned at present
Warden Road	Eastchurch	Site inspected due to ongoing flooding issues. Majority of flooding being caused by field run-off. CCTV survey of highway assets has been carried out.	Works Completed
Otterden Road	Eastling	Drainage improvement works to reduce flooding of carriageway near Eastling Centre	Works Completed
Tunstall Road	Tunstall	Replacement of pond liner and soakaway gravel pack under pond due to damage / wear of liner.	Works Completed
Seed Road	Newnham	Drainage improvement works to reduce flooding near Sandhurst Farm	Works Completed
Wrens Road	Borden	Kerbing works to reduce flood risk to property.	Works Completed

Tanner Street	Faversham	Investigation into alleviation of flooding issues near junction with West Street in heavy rainfall.	Joint working with Southern Water taking place for further measures to reduce flood risk. Topographical survey to be carried out 8 th – 10 th June 2022.
Whitstable Road	Faversham	Investigation into alleviation of flooding issues near Park Row in heavy rainfall.	Joint working with Southern Water. Culvert cleansing carried out following ditch clearance by School by others. All highway drainage in full operational condition - No further works currently planned
Church Road, The Brents	Faversham	Investigation into alleviation of flooding issues caused by tidal water backing up through drains. KCC have undertaken CCTV survey. Road drains link to Southern Water surface water sewers which discharge into the creek via tidal flaps.	All works complete except for internal rendering of 1no. new manhole. With contractor to complete.
Selling Road	Selling	Flooding at low point east of Gushmere Court. Existing drainage discharges into ditch / scrub land at highway edge. Drainage improvement under consideration to implement more suitable drainage layout for easier future maintenance.	Ground investigations completed. With engineers to analyse data and develop new drainage design.
Selling Road	Selling	Flooding affecting area beneath Railway Bridge. Maintenance of drainage system to be carried out. Additional improvements under consideration to reduce flow into Selling Road / Fox Lane from the main road before it reaches the bridge to alleviate flooding.	Ground investigations completed. Existing drainage underwent full cleanse including soakaways 27 th /28 th February 2022.

Roman Square	Sittingbourne	Replacement of 3no. gullies and repair to pipework severed by gas main installation.	Works completed 28 th May 2022.
Tonge Road	Murston	Joint working with schemes team to alleviate flooding issues alongside proposed highway safety improvements.	Site visit held and existing drainage inspected. Engineer to raise job to excavate buried soakaway manholes to allow cleaning / inspection.
Highstreet Road	Hernhill	Works to alleviate long running flooding issue. Highway gully and pipework repairs required. Main issue is silted ditches under third party responsibility requiring digging out.	Ecology survey carried out – additional monitoring required prior to works due to potential for protected species in area of work. Job passed to contractor awaiting ecology requirements report.
Gore Court Road	Sittingbourne	Repair of damaged drainage pipe.	Works Programmed end of May 2022
Fisher Street Road	Baddlesmere	Investigation of highway flooding near Beanys Lane due to blocked drainage	Works programmed late June 2022 under road closure

Appendix C – Street Lighting

Structural testing of KCC owned streetlights has identified the following as requiring replacement. A status of complete identifies that the column replacement has been carried out. Programme dates are identified for those still requiring replacement.

Street Lighting Column Replacement – <i>Contact Officer Sue Kinsella</i>			
Road Name	Location	Description of Works	Status
Kent Avenue	Minster	Replacement of 1 no street light complete with LED Lantern	COMPLETED
Laxton Way	Faversham	Replacement of 1 no street light complete with LED Lantern	COMPLETED
Canterbury Road	Sittingbourne	Replacement of 1 no street light complete with LED Lantern	COMPLETED
Miranda Court	Sheerness	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Eurolink Way	Sittingbourne	Replacement of 2 no street lights complete with LED Lanterns	Works awaiting programming by the end of July 2022
Diamond Court	Sheerness	Replacement of 1 no street light complete with LED Lantern	COMPLETED
Bramley Avenue	Faversham	Replacement of 1 no street light complete with LED Lantern	COMPLETED
Halfway Road	Minster	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Cress Way	Faversham	Replacement of 1 no street light complete with LED Lantern	COMPLETED
Mountview	Borden	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of a July 2022
Church Road	Faversham	Replacement of 3 no street lights complete with LED Lanterns	COMPLETED
Roebuck Road	Faversham	Replacement of 1 no street light complete with LED Lantern	COMPLETED
Forbes Road	Faversham	Replacement of 1 no street light complete with LED Lantern	COMPLETED
The Finches	Sittingbourne	Replacement of 1 no street light complete with LED Lantern	COMPLETED

Miller Close	Sittingbourne	Replacement of 6 no street lights complete with LED Lanterns	Works awaiting programming by the end of July 2022
Vincent Gardens	Sheerness	Replacement of 1 no street light complete with LED Lantern	COMPLETED
Mill Hill	Minster	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Brook Road	Faversham	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Grovehurst Avenue	Sittingbourne	Replacement of 2 no street lights complete with LED Lanterns	Works awaiting programming by the end of July 2022
Highsted Road	Sittingbourne	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Keycol Hill	Bobbing	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Newman Drive	Sittingbourne	Replacement of 5 no street lights complete with LED Lanterns	COMPLETED
Otterham Quay Lane	Upchurch	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
London Road	Teynham	Replacement of 2 no street lights complete with LED Lanterns	COMPLETED
Murton Place	Graveney	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Nautilus Close	Minster	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
South Avenue	Sittingbourne	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Crown Quay Lane	Sittingbourne	Replacement of 2 no street lights complete with LED Lanterns	Works awaiting programming by the end of July 2022
Bonham Drive	Sittingbourne	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Petfield Close	Minster	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Epps Road	Sittingbourne	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022
Chalkwell Road	Sittingbourne	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022

Lower Road	Minster	Replacement of 1 no street light complete with LED Lantern	Works awaiting programming by the end of July 2022

Appendix D – Transportation and Safety schemes

The Schemes Planning & Delivery Team is implementing schemes within the Swale District, to meet Kent County Council’s strategic targets (for example, addressing traffic congestion, or improving road safety). Contact Officer – **Vacant Please contact Schemes Planning and Delivery Team**

CASUALTY REDUCTION MEASURES			
Identified to address a known history of personal injury crashes			
Road Name	Parish	Description of Works	Current Status
A2 London Road JW Faversham Road	Norton, Buckland and Stone	Vegetation Clearance, High friction surfacing and signage	Scheme has been partially completed; road markings to be refreshed
High Street JW Bull Lane	Newington	Speed reduction, implementation of 1 way on Bull Lane	Completed. Some remedial works pending: including speed limit signs on Wardwell Lane
Sheppey Way Bobbing Apple Svcs/McDonalds	Bobbing	New Roundabout Junction to replace existing right turn lane which is currently a priority junction	Scheme currently in outline design. Topographical and Utility Surveys underway.
Queenborough Road JW Belmont Road	Minster on Sea	Road Markings and additional warning signs	Scheme complete. Pending sign off site visit.

INTEGRATED TRANSPORT SCHEMES			
Local Transport Plan funded non-casualty reduction schemes			
Road Name	Parish	Description of Works	Current Status
Tonge Road, Murston	Sittingbourne	Traffic calming scheme	Detailed design stage
Dark Hill/ Stonebridge Pond	Faversham	Footway widening, crossing improvements	Scheme handed over to contractor for delivery. Works date to be agreed with Roadworks Team.
Ospringe Road B2040	Faversham	Footway and dropped crossing improvements at pathway to Faversham Strike Force Football Club	Currently at outline design stage

Forbes Road B2041JW Athelstan Road	Faversham	Zebra Crossing improvements and Junction alterations at Athelstan Road	Currently at outline design stage
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SMALL WORKS SCHEMES Local Transport Plan funded non-casualty reduction schemes			
Road Name	Parish	Description of Works	Current Status
High Street, Sittingbourne	Sittingbourne	20mph speed limit enhancements	Completed
Oad Street, Borden	Borden	Bend warning signs	Completed
Preston Park, Faversham	Faversham	Dropped kerbs at junction with A2	Defects noted (upstand of kerb too steep) contractor to carry out repairs.
Maidstone Road, Danaway	Borden	Speed limit signs	Completed
Avenue of Remembrance	Sittingbourne	Yellow Box road markings and KEEP CLEAR road markings at the exit to the Police Station	Handed over to contractor
A2 London Road near junction of Egbert Road	Faversham	Dropped kerbs	Handed over to contractor

Appendix E – Developer Funded Works

Key:

	Technical Vetting Underway
	S278 Agreement Not Progressed
	Significant Works Outstanding
	Maintenance Period
	Minor Works Outstanding
	Site Adopted Recently

Developer Funded Works (Section 278 Works)				
File Ref.	Road Name	Parish	Description of Works	Current Status
SW/003014	Frognal Lane, Teynham	Teynham	New footway and access to housing development on Frognal Lane	Letter of Agreement in place. Works completed. Remedial works required. Date for remedials TBC by developer
SW/003025	Sheppey Way, Iwade	Iwade	Provision of New Junction/Access for Housing Development	Adoption paperwork completed Feb 2022.
SW/003027	Tunstall Road, Tunstall	Tunstall	New School access Traffic calming changes and footway Connection	Works Completed Serving Maintenance Period – Lighting remedial works. Awaiting confirmation from Developer that these have been completed.
SW/003032	Old Water Works Site, Rook Lane, Keycol, Bobbing	Bobbing	Provision of Revised Footway and Access to Housing Development	Agreement in place. Outstanding remedial works required. H&S File, As-Built Drawings and RSA Stage 3 required.
SW/003035	109-111 Staplehurst Road, Sittingbourne	Sittingbourne	Provision of revised traffic calming and vehicle access for Housing developments	Scheme being progressed by Default S38 & S278 Agreement Specialists. Agreements & Structures awaiting update regarding retaining wall construction details (not as per agreed design). Developer to demonstrate built to adoptable standards. Stage 3 RSA undertaken. Awaiting date for agreed remedial works by developer.
SW/003040	Otterham Quay Lane, Upchurch	Upchurch	Provision of Right Turn Lane / Junction and Footway for Housing Development	Remedial and completion works still required. Awaiting confirmation of date for these.
SW/003041	Larkrise, Conyer Road, Conyer	Teynham	Provision of footway to Small Housing Development	Remedial works still required prior to sign off. Awaiting confirmation of date for these.

SW/003043	34-40 Rushenden Road	Queenborough	Reconstruction of existing lay-by as new Footway	Confirmation of final remedial items having been actioned required from developer. RSA3/H&S File/As-Built Drawings required following completion of remedials.
SW/003046	Power Station Road, Halfway, Sheppey	Minster-on- Sea	Provision of Private Housing development Junction and Traffic Calming	Road Safety Audit Stage 3 undertaken. Remedial works agreed. Awaiting date for remedial works to be carried out prior to Certificate 1.
SW/003048	Parsonage House, School Lane, Newington	Newington	Provision of New Access to Housing site and Traffic Calmed footway crossing	Further remedial works following RSA3 Report to be carried out. Awaiting confirmation of date for works from Developer. TRO to be drafted and consultation progressed. Material Testing Results, H&S file and As-Built Drawings req'd to progress Certificate 1.
SW/003049	Sunny View, Scocles Road, Minster	Minster-on- Sea	Provision of entrance to Private Housing Site	Certificate 2 issued. Adopted 07 March 2022.
SW/003051	Spirit of Sittingbourne SECTION 3 Milton Rd, St Michaels Rd - Town Centre Highway Revisions	Sittingbourne	Provision of Revised Highway Layouts For New Cinema -M/S Car Park-	Minor remedial items carried out. Soft Landscaping Approval received. Drainage CCTV received. Drainage remedial works required. Date for works TBC. Items to be complete satisfactorily prior to sign-off from Drainage Team and Certificate 2.
SW/003053	Barge Way, Kemsley	Sittingbourne	Provision of Revised Access Arm from Existing Roundabout	Certificate 2 issued. Adopted 18 May 2022.
SW/003061	Swale Way/Reams Way, Kemsley, Sittingbourne	NCP	Temporary Access Crossings across Footways for Soil Removal Works	Minor remedial items to be carried out. Date TBC for remedials by developer prior to Cert 2.
SW/003067	Old Brickworks, Western Link, Faversham	Faversham	Provision of New Roundabout Access for Housing Development	Awaiting start date for remedial works.
SW/003068	CRL, Canterbury Road, Sittingbourne	Sittingbourne	Revision of existing footways to proposed Retirement Home frontage	Outstanding remedial works completed acceptably by developer. Awaiting H&S File & As-Built Drawings prior to issuing Certificate 1.
SW/003069	Rushenden Road, Queenborough, Sheppey	Queenborough	Provision of New Access for Housing Development	Footway remedials and street lighting syphers required. RSA Stage 3 undertaken – awaiting report from safety auditors. H&S File & As-Built Drawings also required prior to Certificate 1.

SW/003071	Spirit of Sittingbourne SECTION 5 West St, Station St - Town Centre Highway Revisions	Sittingbourne	Provision of Revised Highway Layouts For New Cinema -M/S Car Park	Certificate 1 issued. End of Maintenance Inspection carried out. Minor remedial items carried out. Awaiting reinstatement of Waiting Restrictions on Station St. Awaiting Drainage CCTV prior to Certificate 2.
SW/003074	School Lane, Bapchild	Bapchild	Provision of Vehicle access and new footway connection for small housing development	Final white lining completed satisfactorily. S278 Cert 1 issued 5 August 2021. Serving Maintenance Period.
SW/003077	Spirit of Sittingbourne SECTION 4 Station St, St Michaels Rd - Town Centre Highway Revisions	Sittingbourne	Provision of Revised Highway Layouts For New Cinema -M/S Car Park-Access Works	Certificate 1 issued. End of Maintenance Inspection carried out. Minor remedial items carried out. Soft Landscaping Approval received. Drainage CCTV received – remedial works required. Awaiting date for works prior to Certificate 2.
SW/003081	Ham Road, Oare Road, Faversham	Faversham	Provision of Access Road to new Housing Development and Revision of Ham Road from Junction	S278 Certificate 1 issued. Street Lighting installation test certificates, Street Lighting Team sign-off, H&S File and As-Built Drawings and minor remedial items still outstanding prior to Certificate 2.
SW/003082	Brogdale Road, Ospringe	Ospringe	Provision of Access Road to new Housing Development	Agreement in place. Works underway.
SW/003084	Eurolink Way, Sittingbourne	NCP	Site access Letter of Agreement plus new controlled crossing scheme	Agreement in place. Works underway.
SW/003085	Brogdale Road, Ospringe	Faversham	Provision of temporary construction access for housing development	S278 Certificate 1 paperwork being progressed. Maintenance period due to start.
SW/003086	Lower Road 3, Teynham	Teynham	Provision of Frontage Footway to small housing development	S278 Certificate 1 issued. Serving Maintenance Period.
SW/003087	A251 Ashford Rd & A2 London Rd, Faversham	Faversham	Provision of Roundabout access to Housing Development	Cert 1 issued. Minor remedial items to be completed (date TBC by developer) prior to issue of Cert 2.
SW/003088	Leysdown Road, Eastchurch, Sheppey	Eastchurch	Provision of revised access for Wind Farm	End of Maintenance Inspection carried out. H&S File, As-Built Drawings req'd prior to issue of Cert 1.
SW/003090	Minster Road, Minster, Sheppey	Minster-on-Sea	Provision of Access for new small Housing Development	Outstanding remedial works. Date TBC by developer.

SW/003091	Eurolink Way, Milton Road, Sittingbourne	Sittingbourne	Footway Access to Retail Development	Certificate 1 issued. End of Maintenance Inspection carried out. Minor remedial items carried out. Soft Landscaping sign-off received. Awaiting Street Lighting Team inspection prior to Certificate 2.
SW/003092	Castle Road, Sittingbourne	Sittingbourne	New Access and footway to Industrial Units	Letter of Agreement in place. Significant remedial works agreed to be carried out. Date for remedials TBC by developer.
SW/003094	Nova, Graveney Road, Faversham	Faversham	Provision of Private Housing development Junction and Pedestrian Crossing	S278 Certificate 2 paperwork being progressed. Adoption imminent.
SW/003097	Crown Quay Lane, Sittingbourne	NCP	New Bell mouth access into Redrow Eurolink Way Site	Agreement in place. Works underway
SW/003100	Love Lane & Graveney Road, Faversham	Faversham	Two new bell mouth accesses - Graveney Road & Love Lane, Faversham	Agreement in place. Works underway
SW/003101	Lower Road, Teynham	Teynham	Provision of Footway for small Housing Development	Technical approval given. Agreement not progressed by developer.
SW/003103	Oak Lane, Upchurch	Upchurch	Traffic Calming/Footway Access to Small Housing Development	Design Technical Submission to be Re-Submitted by the developer's consultant. KCC still awaiting. Technical Acceptance not yet issued.
SW/003104	Spirit of Sittingbourne Section 1 – St Michaels Road	Sittingbourne	Traffic Calming and access to new Housing development	Certificate 1 issued. Minor remedial items to be carried out prior to Certificate 2.
SW/003105	Spirit of Sittingbourne Section 2 – St Michaels Road/Dover Street/Fountain St	Sittingbourne	Traffic Calming and access to new Housing development	Certificate 1 issued. Drainage CCTV received – drainage remedial works required prior to Certificate 2.
SW/003108	Chequers Road, Minster, Sheppey	Minster-on- Sea	Frontage Footway and Access for Small Housing development	Works complete. RSA3 undertaken – remedial works to be agreed. As-Builts/H&S File required prior to Certificate 1.
SW/003109	Spirit of Sittingbourne – Street Lighting Michaels Road/Dover	Sittingbourne	Street Lighting Submission for Overall Sprit of Sittingbourne Schemes	Certificate 2 issued. Adopted 29 April 2022.

	Street/Fountain St Milton Road			
SW/003110	Spirit of Sittingbourne – Retaining Wall Fountain St	Sittingbourne	Fountain Street turning Area Retaining Wall	S278 Certificate 1 issued. Awaiting Structures Team sign-off RE Retaining Wall prior to Certificate 2.
SW/003117	North Street, Milton Regis	Sittingbourne	Permanent School Drop-off facility and Zebra crossing	Majority of planned remedial works completed. Minor remedial works outstanding. As-Built Drawings, H&S File, RSA Stage 3 all still required prior to Certificate 1.
SW/003118	Grovehurst Road, Sittingbourne	Sittingbourne	Provision of Access for new small Housing Development	S278 Certificate 1 issued. End of Maintenance Inspection carried out. Minor remedial works agreed with developer. Awaiting confirmation from developer that these have been carried out.
SW/003141	Stones Farm, Canterbury Road, Bapchild	Bapchild	Traffic Signal Junction and Access for Private Housing Development	S278 Certificate 1 issued 28 March 2022. Serving Maintenance Period.
SW/003196	Church Road, Sittingbourne Golf Centre - Material Movements	Sittingbourne	Addition of passing places on Lomas Road, Church Road for Golf Centre Material Movements	S278 Certificate 1 issued. End of Maintenance Inspection undertaken. Minor remedial works required prior to issue of Certificate 2. Awaiting confirmation from developer remedial works have been carried out.
SW/003199	Swale Way, Great Easthall, Sittingbourne – Toucan	Sittingbourne	Provision of a Toucan Crossing for the Eurolink 5 Industrial Estate development	Technical Vetting underway.
SW/003202	Tylman Place, Graveney Road - LOA	Faversham	Formation of new entrance to development	Agreement in place. Works underway.
SW/003204	Spirit of Sittingbourne SECTION 9 Rear St. Michaels Road (rear of Kwik Fit) LoA	NCP	Re-kerbing and surfacing of link footpath/access drive between Roundabout and Network Rail Car Park	S278 Certificate 1 issued. Serving Maintenance Period.
SW/003260	Leaveland Corner, Faversham	Leaveland	Minor road widening and access for small housing development	End of maintenance remedial works completed. Certificate 2 paperwork being processed. Adoption imminent.

SW/003266	Station Road, Teynham	Teynham	New bellmouth on to station road, footway works, new lining and a build out.	Agreement in place. Works partially complete. Awaiting completion of the physical works as agreed on site with the developer. RSA 3/ As-Builts/H&S File required prior to Certificate 1.
SW/003267	Wises Lane, Sittingbourne	Borden	Amended alignment to Wises Lane for new housing developer and creating new bell mouth for Wises Lane	Agreement in place. Works commencement TBC by developer.
SW/003294	Quinton Road, Sittingbourne	NCP	Mini roundabouts on Quinton Road access to site.	Agreement in place. RSA3 Remedial works required prior to Cert 1
SW/003314	Belgrave Road, Minster-on-Sea	Minster-on-Sea	Widening to existing Belgrave Road prior to proposed S38 highway works relating to access arrangements to new development 146 no. housing development and associated highway works.	S278 Agreement in place. Works underway.
SW/003315	Belgrave Road, Minster-on-Sea	Minster-on-Sea	Temporary sales access	Letter of Agreement in place. Works underway.
SW/003316	The Crescent Signalling, Belgrave Road, Minster-on-Sea	Minster-on-Sea	Signalling and junction improvements	Technical Acceptance issued. Agreement being drafted.
SW/003318	Cooks Lane, Sittingbourne	Milton Regis	Access arrangements for new private housing development.	Final Footway and carriageway resurfacing remedial works required prior to issue of Certificate 1.
SW/003327	Love Lane Commercial Access, Faversham	Faversham	New commercial access bellmouth	Agreement in place. Date of works commencement TBC by developer.
SW/003337	Chequers Road, Minster, Sheppey	Minster-on-Sea	Frontage Footway for Small Housing development	Physical works complete. Awaiting RSA3 remedials to be carried out /As-Builts/H&S File required prior to Certificate 1.
SW/003366	Ham Road, Faversham	Faversham	New bell mouth into development	Agreement in place. Works underway.
SW/003416	The Old School, London Road, Dunkirk	Dunkirk	Bellmouth highway works for proposed Residential Development of 6no. units with associated parking and external	S278 Technical Acceptance granted. S278 Letter of Agreement issued to developer for signing. Awaiting receipt of updated cost of works estimate/ agreement fees/signed

			works.	agreement/developer checklist.
SW/003418	Lydbrook Close, Sittingbourne (junction with London Road/A2)	Sittingbourne	Footway improvement works at the junction of London Road (A2) including footway resurfacing, new kerbing, pedestrian crossing point and minor kerb realignment on the Lydbrook Close nearside approach to London Road.	Signed S278 Letter of Agreement in place. Awaiting confirmation of date works to commence.
SW/003419	The Thanet Way/Dargate Interchange, Hernhill, ME13 9EN	Hernhill	Bellmouth and frontage footway works to facilitate proposed development of 34 commercial units at The Thanet Way/Dargate Interchange, ME13 9EN	Technical Acceptance granted. Awaiting cost of works estimate from developer. Letter of Agreement being drafted.
SW/003420	Aldi, Queenborough Road, Sheppey	Queenborough	Temporary Construction Access for new Aldi Store	Technical Acceptance granted & Letter of Agreement drafting underway. Planning Permission subsequently quashed by Secretary of State – awaiting validated permission from LPA. Awaiting resubmission subject to planning permission being granted.
SW/003422	Staple Street, Hernhill, Faversham	Hernhill	New bell mouth access to 8 dwellings - road to remain private	Agreement in place. Works underway by developer.
SW/003423	The Slips, Scocles Road_Elm Lane, Minster-on-Sea	Minster-on-Sea	New footways, carriageway widening, gateway feature and 2no. bellmouth accesses on Scocles Road to facilitate access to new development of 62 no. residential dwellings.	S278 Agreement in place. Works underway by developer.
SW/003426	Oare Road, Faversham	Faversham	New footway/cycleway link to Oare Road - by traffic signals	Agreement in place. Works start date TBC by developer.
SW/003427	A2 Canterbury Rd J/W Love Lane, Faversham	Faversham	New traffic signal controlled junction	Civils approved. Awaiting signals and lighting sign-off. Agreement being drafted.

SW/003428	Whitstable Rd, Faversham - Zebra	Faversham	New zebra crossing	Civils approved. Awaiting Street Lighting sign-off. Agreement being drafted.
SW/003429	Love Lane, Faversham - Zebra	Faversham	New zebra crossing	S278 Agreement in place. Works due to start June 2022.
SW/003430	Love Lane, Faversham – Bus Stop	Faversham	New Bus Stop layby	S278 Agreement in place. Works commencement TBC.
SW/003432	Scocles Farm, Scocles Road, Minster-on-Sea	Minster-on- Sea	S278 Bellmouth and associated footway works to facilitate access to new development.	Technical Acceptance granted. Agreement being drafted.
SW/003433	Quinton Rd/Sonora Way, Sittingbourne	NCP	2 no. zebra crossings and roundabout improvements	Technical Acceptance granted for Northern Zebra Crossing (between Sapphire Close and Olivine Close). Letter of Agreement signed (northern crossing) and works due to commence imminently. NB Southern Zebra Crossing to be included in separate S278 Agreement which is currently under review.
SW/003435	ATS Site, Crown Quay Lane, Sittingbourne	NCP	New Footway/cycleway	Technical Vetting on-going. Awaiting Street Lighting Team sign-off of the design prior to issuing formal Technical Acceptance.
SW/003436	Chestnut Street, Sittingbourne	Borden	New bellmouth access	Technical Acceptance granted. Agreement being drafted.
SW/003441	Pond Farm, Iwade, Sittingbourne – S278 Sheppey Way and Grovehurst Rd	Iwade	Road improvements on Sheppey way and Grovehurst Road, Sittingbourne, to facilitate access to new residential development at Pond Farm in the parish of Iwade. Sheppey Way traffic calming improvements comprising widening of the existing refuge island and associated road widening. New site access on Grovehurst Road with associated road widening and	Technical Vetting on-going.

			existing footway/cycleway to be extended to 3m in width and continue to Grovehurst Rd roundabout.	
SW/003442	Land at Southsea Avenue, Augustine Rd, Sexburga Drive, Minster-on-Sea	Minster-on-Sea	S278 highway works comprising bellmouth accesses, vehicular crossover accesses and footway works in Augustine Road, Sexburga Drive and Southsea Avenue, Minster-on-Sea, to facilitate residential development comprising 72no. 3- and 4- bedroom dwellings with associated garaging, parking and infrastructure.	Technical Acceptance granted. Agreement drafting in progress.
SW/003446	London Road, Faversham	Faversham	Bus Stop Amendments on A2	Technical Acceptance granted. Agreement drafting in progress.
SW/003453	Chestnut Street, Sittingbourne	Borden	New four arm roundabout - access to A249 and Borden/Wises Lane Development	Technical Vetting underway.
SW/003457	Crown Quay Lane, Sittingbourne	NCP	New Bellmouth access to new housing development	Technical Vetting underway.
SW/003459	Land adjoining Faversham Showground. Staple Street, Hernhill	Graveney with Goodnestone & Boughton under Blean	Construction of a new crematorium, associated car park, access road and gardens of remembrance.	Technical Vetting underway.
SW/003461	Former Regis Medical Centre, Saffron Way, Sittingbourne	NCP	Change of use from temporary doctor's surgery (use now discontinued and building removed) to a restaurant/cafe and hot food takeaway uses, including the erection of 2no. single storey buildings, comprising of 3no. units, and associated car parking, servicing	Technical Vetting underway.

			and landscaping. Creation of access to Milton Creek Country Park.	
SW/003462	Land at Hill Farm, Rook Lane, Kecol Hill, Bobbing	Bobbing	Erection of a nurse accommodation building, car park and outdoor event space for Demelza. Erection of 30 private residential dwellings, together with associated access, parking, highway works, drainage and landscaping. Proposed realignment of Rook Lane.	Technical Vetting underway.
SW/003464	Lower Road, Teynham	Teynham	Provision of footway, layby and 4 vehicle crossings on the north side of Lower Road to the east of Frogal Lane	Technical Vetting underway.

Appendix F – Bridge

Bridge Works – Contact Officer Helen Rowe			
Road Name	Parish	Description of Works	Current Status
Whitstable Road	Faversham	Lady Dane Footbridge (KCC No.3065) bridge refurbishment / replacement works	Feasibility / design option stage.

Appendix G – Traffic Systems

There is a programme of scheduled maintenance to refurbish life expired traffic signal equipment across the county based upon age and fault history. The delivery of these schemes is dependent upon school terms and holiday periods; local residents, businesses and schools will be informed verbally and by a letter drop of the exact dates when known.

Traffic Systems - Contact Officer: Toby Butler		
Location	Description of Works	Current Status
A2 London Road near Chalkwell Road, Sittingbourne (05-0193)	Refurbish existing traffic signal-controlled crossing and convert to near-sided pedestrian facilities	Completed May 2022
B2005 Grovehurst Road near Newman Drive, Sittingbourne (05-0707)	Refurbish existing traffic signal-controlled crossing and convert to near-sided pedestrian facilities	Completed May 2022
A250 Millennium Way near Rose Street, Sheerness (05-0941)	Refurbish existing traffic signal-controlled crossing and convert to near-sided pedestrian facilities	Started May 2022

Appendix H – Combined Member Grant

Combined Member Grant programme update for Swale Borough Council

The following schemes are those which have been approved for funding by both the relevant Member and by the Director of Highways, Transportation and Waste. The list only includes schemes, which are

- in design
- at consultation stage
- Handed over for delivery
- Recently completed on site.

The list is up to date as of **20th May 2022**

The details given below are for highway projects only. This report does not detail

- Contributions Members have made to other groups such as parish councils
- Highway studies
- Traffic / non-motorised user surveys funded by Members.

More information on the schemes listed below can be found by contacting the District Manager for the Swale District.

Cameron Beart

Details of Scheme	Status
21/22-CMG-SW-01 Cowstead Corner New Merge signs	Scheme has been handed over to our contractors for delivery

Appendix I – Public's right of way

Public Rights of Way – Contact Officer – Matthew Fox			
Path No	Parish	Description of Works	Current Status
ZSX62 – End of Seager Road	Sheerness	Add tarmac to existing path	Works complete
ZSX64 – End of Wheatsheaf Gardens	Sheerness	New tarmac path constructed	Works Complete
ZU1	Sittingbourne		Initial planning stages-seeking contribution
ZR547 – Off of Hickmans Road	Dunkirk	New type 1 stone surface	Works Complete
ZR33 – Just off of	Lower Halstow	Repair existing tarmac surface	Planning Stages

The Street			
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Legal Implications

1.1.1 Not applicable.

1.2 Financial and Value for Money Considerations

1.2.1 Not applicable.

1.3 Risk Assessment

1.3.1 Not applicable.

Contacts: Richard Emmett / Alan Blackburn 03000 418181

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SWALE JOINT TRANSPORTATION BOARD (JTB)

Updates are in italics

Minute No	Subject	SBC/ KCC	Recommendations Made by Board	KCC/SBC - Comments/date due back to JTB
634/02/22	Faversham Residents' Parking Scheme – Permit Eligibility	SBC	(1) That Option 2, Administer Permits as Detailed in the Current TRO be approved and progressed.	<i>Parking Administration Team advised – to changes required to current Traffic Regulation Order.</i>
636/06/22	Faversham Local Cycling and Walking Infrastructure Plan (LCWIP)	SBC/KCC	(1) That the contents of the document be noted. (2) That the Local Cycling and Walking Infrastructure Plan (LCWIP) be used as evidence base in SBC and KCC decisions, especially in KCC Highways Strategies and Plans, KCC and SBC planning applications and their associated Section 106s and to inform the SBC Local Plan Review. (3) That the LCWIP be used by both SBC and KCC to assist in applying for funding for active/sustainable travel initiatives.	<i>Recommendations noted by Officers.</i>
637/06/22	The Crescent, Halfway – Traffic Regulation Order	KCC	(1) That the TRO be rejected following the public consultation which showed that local residents were demonstrably opposed to the scheme.	

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Requests made by Councillors and Members of the Swale Joint Transportation Board

Two items were received as listed below:

- Further to my member's question at Full Council last June, I request an update at the next JTB on the Council's investigations as detailed in Cllr Baldock's response to that question, reproduced below for convenience.

"There is a recognition that residents in Great Easthall and Heron Fields need a safer and more environmentally appropriate access to the A2 and the Council have committed to investigating potential opportunities to seek effective connections between Stones Farm and Great East Hall to the A2 that would avoid the considerable increase in traffic pressures on Teynham, Ospringe and Bapchild that traffic modelling has shown a Northern Relief Road would cause."

Also, an update from KCC and from Swale on the traffic modelling referred to in Cllr Baldock's reply, which I understand may have changed since last June, would be very helpful in keeping residents informed.

- As part of the Stones Farm planning permission there was a requirement to install detector loops at the Swanstree Ave junction and the need for a revised junction layout. The new houses at Stones Farm are now being occupied and yet the junction upgrade appears not to have been completed.

Please can we have an update on the upgrade of the Swanstree Avenue junction with A2. At present there is a significant issue with standing traffic through Bapchild all the way back to Hempstead House Hotel and along the A2 into/out Sittingbourne.

I understand "A planning application on 'Land Between Frogal Lane And Orchard View, Teynham' (ref. 16/507689/OUT), which was approved in June 2021 (and which itself includes a requirement for a contribution to the Swanstree Ave Junction Works), also includes a drawing indicating what the Improvement Works are. "

Please can KCC explain what has been done to upgrade the junction to date , when will the detector loops be switched on and when the revised layout design be finalised, constructed and implemented.

Response

KCC's Schemes Planning Delivery team are looking into the two items submitted and outlined below and agreed to the Members directly to discuss further. It was agreed that they would not be considered at the June JTB meeting

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